

Carbon County Road and Bridge Management Policy

Adopted February 16, 2021
Amended January 16, 2024

RESOLUTION NO. 2021-07

**RESOLUTION FOR THE ADOPTION OF A ROAD MANAGEMENT POLICY FOR
CARBON COUNTY,**

WHEREAS, the Board of County Commissioners have the authority to perform duties respecting roads as required by law, and county roads are under the supervision, management, and control of the Board of County Commissioners pursuant to Wyoming Statutes §18-3-504(a)(vi), and §24-1-104; and

WHEREAS, the Board of County Commissioners is responsible for funding the maintenance of county roads and bridges, and constructing, maintaining, and supervising county road standards pursuant to Wyoming Statute §24-2-110; and

WHEREAS, adoption and establishment of a Road and Bridge Management policy would allow Carbon County to provide the public notification of any road standards, classifications, and requirements adopted for Carbon County roads, as well as set consistent standards and maintenance schedules to be followed on county roads throughout Carbon County; and

WHEREAS, the Board of County of Commissioners find it necessary to maintain consistent road standards to promote public health, safety, and general welfare of its citizenry, and find it necessary to maintain certain standards regarding county road construction and maintenance; and

WHEREAS, a public hearing was held on the 16th day of February 2021, in accordance with Wyoming Statute §16-3-101, et. al; and

WHEREAS, Wyoming Statute §16-3-103 authorizes the adoption of such rules and regulations; and

WHEREAS, the Carbon County Board of County Commissioners find it reasonable and necessary to regulate road construction, maintenance and associated work in accordance with Wyoming Statutes §18-3-504, §18-3-701, §24-3-201 *et. seq.*, §31-5-108, and §31-5-112; and

WHEREAS, the Carbon County Board of County Commissioners find it reasonable and necessary to regulate road construction, maintenance and associated work in accordance with Wyoming Statutes 18-3-504, 18-3-701, 24-3-201 et. Seq. 31-5-108 and 31-5-112; and

NOW, THEREFORE, BE IT RESOLVED, by the Carbon County Board of County Commissioners of Carbon County adopt the Carbon County Road and Bridge Management Policy, which provides specific procedures for the orderly and consistent management, construction and maintenance of County Roads throughout Carbon County.

This resolution, Resolution No. 2021-07 and regulations hereafter adopted/modified under the provisions of the named resolutions shall repeal and supersede previous resolutions relating to road management in unincorporated Carbon County.

APPROVED, PASSED AND ADOPTED THIS 16TH DAY OF February 2021.

THE BOARD OF COMMISSIONERS,
CARBON COUNTY, WYOMING
By: -s- Willing "John" Johnson
Chairman, Carbon County Commissioner

Attest: -s- Gwynn Bartlett
Carbon County Clerk

Introduction

The objective of the Carbon County Road and Bridge Department is to construct, maintain, and repair our county roads under the classification list provided below to the best of our ability.

It is a constant struggle to keep up with increased traffic volume, larger and heavier vehicles, and development that puts more demand on some roads. The Carbon County Road and Bridge Department will continue to do all it can to give attention to the needs of county roads and related structures, while balancing those needs against the limitations of budgets, increasing costs, less manpower than in the past, and tighter schedules.

It is important for you to know that life in the county is different from life in the city due to the need for resources being shared across rural properties in a broader area. County governments are unable to provide the same level of service that city governments provide. We welcome you to Carbon County and hope that you will enjoy living here but want to assist you in making an educated and informed decision when purchasing rural land.

Access

The fact that you can drive to your property does not necessarily guarantee that you, your guest(s), and emergency service vehicles can always achieve that same level of access. Please consider:

1. Emergency response times (sheriff, fire suppression, medical care, etc.) cannot be guaranteed. Under some extreme conditions, you may find that emergency response is extremely slow.
2. There can be problems with the legal aspects of access, especially if you gain access across property belonging to others. It is wise to obtain legal advice and understand that obtaining legal, insurable easements may be necessary when these types of questions arise.
3. Many large construction vehicles cannot navigate small, narrow roads. If you plan to build, it is prudent to check into construction access.
4. You may experience problems with the maintenance and cost of maintenance of your road. Carbon County has over 950 miles of roads. There are some county roads that are not maintained by the county, with no grading, and no snow plowing. There are even some public roads that are not maintained by anyone! Make sure to know what type of maintenance is provided ~~to expect~~ and who will provide that service.
5. In extreme weather, even year-round county-maintained roads may become impassable. You may need a four-wheel drive vehicle with chains for all four wheels to travel during those instances, which could last for several days. At times it may be impossible for emergency vehicles (sheriff, fire, ambulances) to reach your property. The more inaccessible your property, the more likely this is to occur.
6. Unpaved roads generate dust when traffic levels and moisture conditions reach a certain point. Carbon County does treat some high traffic roads with dust abatement treatment. The county Road and Bridge superintendent and supervisors make the decision on what roads receive treatment. Some residents enjoy the treatment, and some do not. The county will only stop treatment if 51% of the property owners along a county road come forward asking that we stop application. The application of dust abatement aids the county in maintenance by holding the roads together and keeping the dirt on the road.

7. Unpaved roads are not always smooth and are often slippery when they are wet. You will experience an increase in vehicle maintenance costs when you regularly travel on rural county roads.
8. School buses travel only on maintained county roads that have been designated as school bus routes by the school district. You may need to drive children to the nearest bus route so they can get to school.
9. Mail delivery is not available to all areas of the county. Ask the postmaster to describe the delivery system in your area.

County Taxes

All county residents pay taxes to the county government which are used to fund services offered by the county. However, the revenue collected from rural residential developments does not cover the cost of the services provided to rural residents. In general, city residents and rural agricultural producers subsidize the lifestyle of rural residents by making up the shortfall between the cost of services and revenues received from rural dwellers.

If you choose to live in the county, please be aware that the amount you pay to the county government does not cover the services that the county provides for you. With limited funds, the county often cannot afford to provide all the services you might desire.

Carbon County Road & Bridge
County Roads and Classification

Road No.	Road Name and Classification	Begin M.P.	End M.P.	Township	Range	Maintenance Type
1	Medicine Bow-McFadden Road	0	20.9	22	78	Year-round MP 0.0 - 10.0
2W	Shirley Ridge Road	0	7.9	27	80	Seasonal MP 0.0 -7.9
2E	Shirley Ridge Road	0	8.8	27	78	Year-round MP 0.0 - 8.8
3	Elk Mountain-Medicine Bow Road	0	20.49	20	80	Year-round MP 0.0 - 20.49
11	McFadden Fire Station	0	0.1	20	78	Year-round MP 0.0 - 0.1
15	Bengough Hill/Cooper Cove Road	0	11.4	18	77	Seasonal MP 1.2 – 11.4
24	Chalk Hill Road	0	1.5	27	77	Year-round MP 0.0 - 1.5
75	Cheesbrough/Richards Lane	0	1	22	78	Seasonal MP 0.0-1.0
97	Utah Mining/Shirley-Casper Road	19.70	22.24	27	77	
97	Utah Mining/Shirley-Casper Road	26.7	36.07	28	78	Seasonal MP 4.4 -9.3
99	Little Medicine Mail Road	0	14.4	25	78	Seasonal MP 0.0 - 14.4
100	Ferris Road	0	14.2	25	89	Seasonal MP 0.0-14.2
101	Medicine Bow Ranger Station	0	11.7	20	80	Seasonal MP 6.0 - 11.7
102	Leo-Shirley Basin Road	0	21.7	26	83	Seasonal MP 0.0 - 21.7
103	Dry Creek Road	0	9.5	26	81	Seasonal MP 0.0 -9.5
109	Elk Mtn. Cemetery Road	0	0.6	20	80	Year-round MP 0.0 - 0.6
115	Elk Mountain/Carbon Road	0	14.66	21	80	Seasonal MP 1.0 - 14.66
121	Difficulty Shirley Road	0	16.6	23	79	Year-round MP 0.0 - 16.6
191	Troublesome Road	0	3.00	24	81	Seasonal MP 0.0 -3.0
201N	Indian Creek Road North	0	3	14	83	Year-round MP 0.0 - 3.0
201S	Indian Creek Road South	0	2.9	14	83	Year-round MP 0.0 - 2.9
203	Brush Creek Road	0	7.62	16	82	Year-round MP 0.0 - 7.62

205	Lake Creek Road/205 Extension	0	4.63	17	83	Year-round MP 0.0 - 4.63
207	Fish Hatchery Road	0	3.11	18	84	Year-round MP 0.0 - 3.11
209	Cedar Ridge Road	0	8.12	17	83	Year-round MP 0.0 - 8.12
211	Black Hall Road	0	6.67	14	83	Seasonal MP 4.1 -6.6
213	Beaver Creek Road	0	2.45	14	82	Year Round/Limited Maintenance
215S	Stock Driveway	0	13.94	19	83	Seasonal MP 0.9 – 7.8 YR 7.8 -13.9
215N	Stock Driveway	0	1.15	19	83	Year-round MP 0.0 - 1.15
219	Cherokee Trail/Sierra Madre	0	2.74	14	82	Year-round MP 0.0 - 2.74
223	McIlvain Lane	0	0.5	18	83	Year-round MP 0.0 - 0.5
262	Marshall Road	0	7.51	23	78	Year-round MP 0.0 - 7.51
270	Difficulty/Troublesome Road	0	5.8	24	81	Year-round MP 0.0 - 5.8
272	Stone Ranch Road	0	1.1	24	87	Seasonal Access only 0.0 -1.1
285	Mountain View Cemetery Road	0	0.52	15	83	Year-round MP 0.0 - 0.52
291	Hanna Leo, Kortes Road	0	56.69	22	81	Year-round MP 0.0 56.69
294	Oil Springs Road	0	4.86	23	78	Year-round MP 0.0 - 4.86
297	Bill Coffman Road	0	1.5	22	81	Year-round MP 0.0 - 1.5
301	Leavengood Lane	0	0.9	17	84	Year-round MP 0.0 - 0.9
303	Cow Creek/Silver Spur Road	0	12.01	16	83	Year-round MP 0.0 - 12.01
305	Castle Basin Road	0	3.7	28	85	Seasonal Access only 0.0 -3.7
307	Mountain View Estates Road	0	1.07	18	83	Year-round MP 0.0 - 1.07
316W	Old Lincoln Highway	0	6.9	22	81	Seasonal MP 2.0 -6.9
320	Kyle Oil Camp Road	0	8.56	21	79	Year-round MP 0.0 - 8.56
324	Sinclair-Platt River/Golf Course	0	5.17	21	86	Year-round MP 0.0 - 5.17

340	Ferris Crossing Road	0	1.28	21	88	Year-round MP 0.0 - 1.28
345	Savage Ranch Road	0	5.1	20	85	Seasonal MP 0.0 -5.1 Access Only
347N	Savage Ranch Road	0	0.90	21	85	Year-round MP 0.0 - 0.90
347S	Savage Ranch Road	0	7.14	21	85	Seasonal MP 0.0 - 7.14 Access Only
351	Seminoe Road	0	30.97	21	86	Year-round MP 0.0 - 30.97
353	Hog Park/Finley Hill Road	0	6.60	14	84	Seasonal MP 1.0 - 6.60
385	North Spring Creek Road	0	15.9	17	84	Seasonal MP 10.25 - 15.9
387	South Spring Creek Road	0	16.5	17	84	Seasonal MP 12.01 - 16.5
400	Rattle Snake Road	0	20.3	20	84	Seasonal MP 3.26 - 20.3
401	Sage Creek Road	0	45.53	19	88	Seasonal MP 5.0 - 45.5
402	Elk Mountain- Arlington/Wagon	0	13.8	20	80	Seasonal – ACCESS ONLY
404	Pass Creek Road	0	28.72	19	84	Year-round MP 0.0 - 28.72
406	Midway Road	0	3.19	19	84	Seasonal MP 0.0 - 3.19
407	C.I.G. Road	0	1.00	21	86	Year-round MP 0.0 - 1.0
408	Bolton Road	0	10.25	17	84	Seasonal MP 3.45 - 10.25
412E	Oberg Pass Road	0	1.8	18	81	Year-round MP 0.0 - 1.8 Access
412W	Oberg Pass Road	0	3.0	18	83	Only/Seasonal MP 0.0 -3.0
440	McFadden/Rock River Road	0	1.55	19	78	Seasonal/access
440	McFadden/Rock River Road	1.55	2.39	19	77	Seasonal/access
451	Cherokee Road	0	1.3	21	88	Year-round MP 0.0 - 1.3
488	Energy Road	0	1.00	18	80	Seasonal /Access only MP 0.0 -1.0
497	Buzzard/Willow Road	0	42.1	23	88	Seasonal/Access only MP 0 – 42.1

499	North Ferris	0	15.0	27	86	Seasonal/Access only MP 0 -15
500	Jack Creek Road	0	23.63	17	84	Seasonal MP 11.1 - 23.63
501	Cherry Grove Road	0	8.6	12	89	Seasonal MP 4.3 - 8.6
503	McCarty Canyon Road	0	37.62	12	90	Seasonal MP 1.0 - 37.62
504	Saratoga/10 Mile/Ryan Park	0	15.3	17	84	Seasonal MP 11.2 - 15.3
505E	Miller Hill E. Road	0	7.0	17	88	Seasonal MP 0.0 -7.0
505W	Miller Hill W. Road	0	18.40	17	89	Seasonal MP 0.0 - 18.40
508	Pick Bridge Road	0	6.91	18	84	Seasonal MP 3.0 - 6.91
514	Jack Creek School Bus Road	0	2.93	17	84	Year-round MP 0.0 - 2.93
550	Buck Creek Road/205 Connector	0	2.01	18	84	Year-round MP 0.0 - 2.01
561N	Savery North Road	0	8.0	12	89	Year-round MP 0.0 - 8.0
561S	Savery South Road	0	0.40	12	89	Year-round MP 0.0 - 0.40
601	Russell Road	0	1.6	12	90	Year-round MP 0.0 - 1.6
602	Browns Hill Road	0	17.0	14	90	Seasonal MP 0.0 - 17.0
603	Four Mile Road	0	3.64	12	91	Year-round MP 0.0 - 3.64
604	Wolford Road	0	6.07	16	84	Year-round MP 0.0 - 6.07
605N	Twenty Mile/J.O. Road	0	24.35	21	88	Seasonal MP 2.0 - 24.35
605S	Twenty Mile/J.O. Road	38.00	39.5			Seasonal MP 0.0 -1.5
607	Dolan Lane	0	2.0	12	90	Seasonal MP 1.2 -2.0
608	Wild Cow Lane	0	22.5	16	92	Seasonal MP 0.0 - 22.5
613	Baggs-Craig Road	0	1.21	12	91	Year-round MP 0.0 - 1.21
615	Webber Mesa Road	0	0.5	12	91	Year-round MP 0.0 - 0.5
640	North Spring Creek Spur	0	10.3	17	85	Year-round MP 0.0 - 10.3

648	Finch Road	0.00	2.00	14	83	Year-round MP 0.0 - 2.0
660	Holmes/French Creek Road	0	14.33	15	83	Minimal Plowing MP 10.21 -14.33
680	Baggott Rocks Road	0	4.0	15	83	Year-round MP 0.0 - 4.0
684	Sidley Spur Road	0	.91	15	83	Year-round MP 0.0 - 0.91
700	Poison Butte/Government Road	0	12.9	13	91	Seasonal MP 8.0 - 12.9
701	Wamsutter Road	0	19.13	16	92	Year-round MP 0.0 - 19.13
702	Baggs-Dixon Road	0	7.32	13	91	Year-round MP 0.0 - 7.32
703	Heeley Road	0.00	0.50	12	93	Year-round MP 0.0 - 0.50
704N	Baggs-Dixon-Savery North Road	0	0.03	12	91	Year-round MP 0.0 - 0.38
704S	Baggs-Dixon-Savery South Road	0.00	0.30	12	91	Year-round MP 0.0 - 0.3
706	Big Gulch Road	0	6.11	13	89	Seasonal MP 2.0 - 6.11
708	Unbrella Road	0.00	0.40	12	88	Limited Maintenance (private Drive)
710	Snake River Spur	0	6.5	12	88	Year-round MP 0.0 - 6.5
718	Beaver Creek Road	0	1.56	14	82	Year-round MP 0.0 1.56
722	Sheehan Lane	0	2.28	12	91	Year-round MP 0.0 - 2.28
730	Paintbrush Road	0	4.2	12	92	Year-round MP 0.0 - 4.2
752	Stock Drive Road	0	5.0	14	90	Seasonal MP 0.0 -5.0
754	Sandstone Stock Drive Road	0	3.27	13	89	Seasonal MP 0.1 - 3.27
772	Four Mile Extension	0	0.9	12	91	Year-round MP 0.0 - 0.9
798	Holroyd Road	0	4.38	12	80	Seasonal MP 0.0 - 4.38
730N	Paintbrush North	0	2.15	12	92	Seasonal access 0.0 - 2.15
Total Miles year-round 955.89			Total Miles in winter 491.30 x 2 passes 982.60			

Road Classification

Category One	High Priority - Paved and/or Quality Graveled surfaced roads with high traffic volume, open and maintained year-round. Priority for signage, magnesium chloride, bridge/culvert/asphalt repairs, grading, and snow removal.
Category Two	Medium Priority - Good gravel or recycled asphalt surfaced roads. Portions of these roads are open and maintained year-round.
Category Three	Low Priority - Lower quality road with some gravel surfacing. Not a priority and less frequently maintained. Low traffic volume. Mostly seasonal roads and not maintained year-round.
Category Four	No Priority - Unimproved two-track or trail. Historically has had little to no maintenance work done by the county, but it is on county easement. Roads exist only on paper, having NO maintenance or snow removal to be done by the county. Any work would need approval by Road & Bridge Superintendent. Would not be considered for improvements, without significant change or circumstances and approval by the Board of Carbon County Commissioners. Access only road, travel at our own risk.

As of the date of this policy, the County has over 955 miles of county roads throughout Carbon County. Of that, 491 miles are maintained year-round. Most of which consist of a graded, crowned road top with a graveled surface making them category one or two roads. Approximately 90 miles of the 491 miles consists of asphalt-type surfaces, leaving 361.69 miles of category two or three roads, and the remaining 102.31 miles as category four roads.

Along our county roads, we also maintain 2,468 culverts – 900 signs – 452 cattle guards – 51 Bridges.

We have 7 County Shops throughout Carbon County and 1 Main Office.

Carbon County Road & Bridge Main Office
PO Box 487
Rawlins, WY 82301
307-324-9555

Baggs Shop – 307-383-6432
Encampment Shop – 307-327-5767
Elk Mountain Shop – 307-348-7392
Saratoga Shop – 307-326-5681
Medicine Bow Shop – 307-379-2286
Hanna Shop – 307-325-6820
Rawlins Shop – 307-324-9577

1 fulltime Road & Bridge Coordinator
2 fulltime Road & Bridge District Supervisors
10 fulltime Road & Bridge Blade Operators

The purpose of this Carbon County Road Management Policy hereinafter referred to as "this policy," "these standards" is to provide specific procedures for the orderly and consistent management of County Roads throughout the County. This policy provides the Board of County Commissioners, hereinafter referred to as "the Board," assurance that fair and equitable treatment is exercised in providing and prioritizing County Road services upon a repeatable, logical process.

The guidelines for County staff set forth in this policy are meant to be illustrative, not exclusive, descriptions of proper techniques for management of County assets, and shall not be relied upon to establish a standard of care or other determination that a particular technique of asset management is the only technique appropriate under the circumstances. The standards set forth herein shall, however, be utilized as the basis for issuance of permits and any other regulatory provisions set forth in this policy. This distinction recognizes the fact that certain provisions of this policy address actions taken by the County in its proprietary capacity (e.g., asset management by staff), while other policy provisions address actions taken by the County in its regulatory capacity (e.g., permitting and enforcement of same). Nothing in this policy shall be construed to create a private right of action of any kind, including without limitation, a private right to enforce the provisions of this policy.

Objectives

The objectives of this policy are as follows:

1. Communicate best asset management practices.
2. Provide guidelines for the maintenance of all county roads in Carbon County.
3. Provide guidelines for all permitting pertaining to County Roads and the Road and Bridge Dept.
4. Assist in the administration and maintenance of County assets.
5. Clarify maintenance levels of service and apply them to assets.
6. Standardize construction activities.
7. Provide guidelines for all aspects of construction.
8. Outline permitting requirements that parties, other than the County, must satisfy to utilize or undertake certain activities that may affect County assets.
9. Provide guidelines for staff to review private development requests.
10. Develop a baseline for the allocation of funding and resources for roadway maintenance within the County. The policy is designed to ensure that the County can better meet the expectations of the public when it comes to maintaining county roads by establishing a long-term outlook.
11. Preserve the County's capital investment in paved roadways by providing proper and timely maintenance.
12. Propose the most cost-effective treatment options for County roads according to their current condition.
13. Provide a systematic way to budget for the maintenance of County roads.
14. Re-evaluate roadway conditions and establish long-term benchmarks to be met by future plans.

This policy is intended to aid the County in making cost-effective decisions that preserve and improve current roadways and properties. This policy shall be reviewed periodically to remain current with the expanding needs and expectations of the County.

Applicability

This document applies to all County roads and structures located within the unincorporated areas of Carbon County, Wyoming.

Statutory Authority

The State of Wyoming, through the following statutes authorizes the Board to administer the County's assets related to roads, access and addressing.

- W.S. § 16-3-103 allows for the adoption of rules that describe the organization, procedures, or practice requirements of any agency.
- W.S. §18-2-101, "General Powers" and §18-3-504, "Powers and duties generally", authorizes the Board of County Commissioners to manage the business and concerns of the County in the exercise of its corporate or administrative powers.
- W.S. §18-3-504(a)(vi) pertains to the layout, alteration or discontinuance of roads running through the county.
- 18-3-506 allows commissioners to establish rules and regulations to govern the transaction of their business.
- W.S. §18-5-201, et seq, provides for a county's authority over the location and use of buildings and structures and the use and occupancy of lands.
- W.S. §18-5-306 (a)(vii) provides for a county's authority over subdivision roads.
- W. S. §24-1-104: "Management and Control of County Roads," authorizes the Board of County Commissioners to administer the County Road system including, but not limited to, maintenance, layout, establishment, alterations, vacations, property acquisition and traffic regulations.
- W. S. §24-3-101, et seq. outlines the authority and procedures to be followed in the establishment, vacation, or alteration of County highways. This policy is considered a minimum guideline and is not intended to replace or to conflict with this nor any other federal or state law or regulation. Should there be a conflict, the more stringent law, regulation, or statute shall apply.
- W.S. §§24-6-101through 111provides counties with additional authority over access issues related to public highways.
- W.S. §35-10-401 provides that public highway, including county road rights of-way, shall not be obstructed, and provides for criminal penalties.
- W.S. §§6-6-301 through 307, prohibits unlawful conduct within governmental facilities, including land under the control of a governmental body.

This policy establishes certain rules, standards and guidelines that have the force of law. Violations of these rules are punishable as provided herein or as established by Wyoming State Statutes, as amended.

Delegation of Authority

Road Standards

- The Board delegates the County Road & Bridge Superintendent as the authority to administer and enforce Chapter 3 of this policy.
- The Board delegates the County Road & Bridge Superintendent and the County Road and

Bridge Foreman as the authorities to review Permits, examine road-related issues, and conduct inspections.

Repeal of Conflicting Provisions

This policy is not intended to repeal, abrogate, or impair any existing easements, covenants, or deed restrictions. Where this policy and another policy or regulation, easement, covenant, or deed restriction conflict or overlap, whichever imposes the more stringent restrictions shall prevail.

Interpretation

In the interpretation of this policy, all provisions shall be:

1. Considered as minimum requirements.
2. Liberally construed in favor of the governing body; and
3. Deemed neither to limit nor repeal any other powers granted under state statute.

Severability

This policy is hereby declared to be severable. Should any portion of this policy be declared invalid by a court of competent jurisdiction, the remaining provisions shall continue in full force and effect and shall be read to carry out the purpose(s) of this policy before the declaration of partial invalidity.

Warning and Disclaimer of Liability

This policy shall not create liability on the part of Carbon County, or any officer or employee thereof, for any damages that result from reliance on this policy, or any administrative decision lawfully made in furtherance of them.

CHAPTER 2. DEFINITIONS AND TERMS

Acronyms

ADT	Average Daily Traffic
MUTCD	Manual on Uniform Traffic Control Devices
PLSS	Public Land Survey System
WPWSS	Wyoming Public Works Standard Specifications
WYDOT	Wyoming Department of Transportation

Terms

Access - an entrance or exit to another public roadway or to private or public land from a street or highway.

Address - a numerical symbol identifying a residence, business or other facility that is located along an access.

Asset - any County-owned or maintained road, right-of-way, trail, facility, vehicle, equipment, or property.

Bridge - a structure erected over a depression or an obstruction, such as a waterway, highway, or railway, having a passageway for carrying traffic and an opening, measured along the center of the roadway, of more than 20 feet.

Clear Span - the distance measured along centerline from the front face of a support to the front face of the next support in a bridge structure.

Contractor - any person or organization who is party to a contract with the County to construct, renovate, or perform major maintenance of any road, public road, bridge, public improvement, structure, or system.

County Road - any road or right-of-way established by the County and according to state statute within the jurisdiction of Carbon County

Culvert - A structure other than a bridge that creates a transverse opening under a roadway or embankment for drainage or similar purposes.

Easement - a right of use over the property of another; the right to use the real property of another for a specific purpose.

Materials - All components required for construction of the project.

Pavement -The layers of oil-based materials placed on a subgrade to support and distribute the traffic load to the roadbed. Needs to include oil and a double-invert road treatment (back-to-back layers of chip seal) as well as asphalt overlay.

Paved - means a roadway which is covered by hot-rolled asphalt or concrete but is not constructed solely of recycled asphalt.

Private road - a roadway for the use of an individual or group of individuals; every way or place in private ownership and used for vehicular travel by the owner and those having express or implied permission from the owner, but not by other persons.

Professional engineer - A person registered and authorized to practice in one or more branches of engineering by the Wyoming State Board of Registration for Professional Engineers and Professional Land Surveyors. When the expertise of a particular branch of engineering is specified (for example, civil engineering), the County requires registration in that discipline.

Public road - a road right-of-way or easement dedicated or established for the use of the general-public; not to be construed as a County Road.

Residence - a house or facility which may be any structure, such as a building, or temporary, such as a trailer which is occupied/lived-in for some continuance of time on a parcel of property.

Right-of-Way - the legal right, established by usage or grant, to pass along a specific route through grounds or property belonging to another.

Road - a general term denoting a public usage way used for vehicular travel, including the entire area within the right-of-way.

Roadbed - the graded portion of highway within the top and side slopes, prepared as a foundation for the pavement structure.

Shoulder - The portion of the highway, street, or road next to the traveled way. Provided for stopped vehicles, emergency use and lateral support of the base and surface covers.

State Highway - a right-of-way established according to statute by the State of Wyoming, under the jurisdiction of the State of Wyoming.

Subdivision - The creation or division of a lot, tract, or parcel or other unit of land for the immediate or future purpose of sale, building development or redevelopment, for residential, recreational, industrial, commercial, or public uses. The word subdivide or any derivative thereof shall have reference to the term subdivision, including mobile home courts, the creation of which constitutes a subdivision of land.

Subdivision Road-A Road, right-of-way, or easement dedicated or established through the subdivision process.

Subgrade - the top of a roadbed upon which road surface and shoulders are built.

Unmanned facilities - facilities that are not considered places of residence or business.

CHAPTER 3 ROADS, BRIDGES AND OTHER DEVELOPMENT IN COUNTY RIGHTS-OF-WAY

DECLARATION OF COUNTY ROADS

County roads are all roads in the County that have been established pursuant to the laws of the State of Wyoming and/or are accepted by the Board.

CLASSIFICATION AND NAMING OF ROADS

Roads contained in the County-wide circulation system are classified based on their functional use and traffic volumes. Ownership of the road rights-of-way and the party responsible for maintenance are determined in accordance with state statutes or controlled by the Board.

Establishment of a road right-of-way does not guarantee the road is eligible for maintenance or that it is or will be maintained by the County. See County Road classifications listed above on page 6.

Primary or Arterial Roads

Primary or arterial roads are generally through roads carrying traffic to and from major areas of population with minimal interference and are normally high-speed roads containing all signage and road markings to accommodate higher volumes of traffic. Average Daily Traffic (ADT) counts for primary or arterial roads are greater than 700 (>700) vehicles per day (see *Table 1: Design Capacity of Classes of Roadways*). These types of roadways are most often under WYDOT supervision and authority within the County. The County itself does not currently have any County Roads that meet this classification.

Secondary or Collector Roads

Secondary roads serve as collectors of traffic from residential, industrial, agricultural, and recreational areas to the primary road system. Secondary roads provide a link between local roads and arterial roads. Secondary roads should be designed so that they do not disrupt the activities and land uses they serve. Secondary roads should provide for relatively high overall travel speeds. In addition, access to secondary roads should be designed to minimize interruption of traffic flows. Typically, the ADT is greater than or equal to 100 and less than or equal to 700 vehicles per day. The County has numerous roads that qualify for this classification. Secondary roads are characteristically an asphalt paved surface.

Residential or Local Roads

Residential or local roads provide access to primary and secondary roads for industry, service vehicles, agriculture, recreationists, and the general-public. These roads may be of an asphalt surface or an all-season graveled surface with adequate drainage and signage to direct safe usage by the general-public. Most County Roads fall within this classification. The typical ADT for residential or local roads is 99 vehicles per day.

Primitive or Two-Track Roads

Any road or trail pioneered by and for a local purpose and that primarily serves as minimal access to outlying areas for recreation, ranching, industry, or privacy issues would fall within this classification. This type of road is typically seasonal and probably receives little or no regular road maintenance.

ROAD TYPES BASED UPON OWNERSHIP AND MAINTENANCE

Examples of road types are as follows:

- **County Roads:** Under this category, the County, through the Board and/or in accordance with State Statute, has accepted the road rights-of-way and has assumed responsibility for construction and road maintenance.
- **County Road Rights-of-Way Not Maintained by County:** In some instances, the County has accepted road rights-of-way but does not routinely maintain the roadway. These rights-of-way are available for the use of the public or reserved for the future needs of the County.
- **Private Roads:** This category includes all roads where the adjacent property owners retain ownership of the road right-of-way either through a deed, easement, or covenants, and they generally hold responsibility for its maintenance. Included are subdivision roads, whether private or available for public travel. The County, generally, does not maintain any private roads. Exceptions may exist, such as a through road serving other County residents or businesses beyond a subdivision.

ROAD NAMING

A named road does not imply that it is an established County Road, not that it is maintained by the County. (i.e., platted subdivision roads or lanes and roads never adopted as County roads). However, the name or number assigned to each road does identify it for rural addressing purposes.

ROAD AND BRIDGE DESIGN AND CONSTRUCTION STANDARDS

Overview

This section sets forth general standards for roadway and bridge design in the County and is intended for use by design engineers and developers. While not all county roads are 100% in compliance with these standards currently, it is the County's desire to bring all category one roads into compliance subject to existing geographical conditions and budget limitations.

The basis for design used in these standards is the American Association of State Highway and Transportation Officials' (AASHTO) "A Policy on the Geometric Design of Highways and Streets."

The basis for construction used in these standards is the Wyoming Public Works Standard Specifications Federal Highway Administration -Gravel Roads Construction and Maintenance Specifications Wyoming Department of Transportation Earthwork Standard Plan.

Alternatively, applicable specifications of other agencies or organizations may be incorporated into designs provided they are approved in advance by and at the sole discretion of the County.

PLANNING FOR THE FUTURE

Prior to the design of a new road or upgrade/redesign of an existing road, projections of future development and densities, estimates of future traffic volumes and appropriate classifications will be determined by the Road and Bridge Superintendent. All speed index surveys shall be completed by a qualified engineer. The road classification determines the geometric cross section and maximum sustained grades, while the design speed determines minimum or maximum standards for elements of alignment such as stopping and passing sight distances, radii of curvature, tangent lengths, and super elevation transition lengths.

The following are minimum considerations for road planning:

- Design period: Roadway design shall be based on the projected needs twenty (20) years after construction.
- Projected development: Projections of development over the design period shall be based on existing land use, proximity to developed areas, historic growth, and other factors, such as County or municipal development plans which can be expected to influence development.
- Projected traffic volumes: For example, residential property generates an ADT count of ten (10) trips per unit. These per unit ADT counts are applied to the projected development to generate estimates of the design year traffic volumes. When per unit ADT counts are not listed for a type of development, or an ADT has not been established for a particular category or location by the County, the design engineer shall use an acceptable reference approved by the County, such as the Institute of Transportation Engineer's (ITE) *Trip Generation Manual* (current edition), to obtain the appropriate ADT count. Traffic volumes will be projected following common accepted practices.
- Access to subdivisions shall be in accordance with the current County subdivision regulations.
- Road rights-of-way shall be provided from proposed subdivision roads or streets to adjacent or adjoining lands if such easements or rights-of-way would improve access to the lands for potential development or potentially landlocked lands and would facilitate the development of a coordinated road system developing within the area.
- Roads shall be designed and aligned to join with planned or existing roads.

Minimum Road Design and Construction Standards

Table 3: Summary/Minimum Road Design Elements provides a summary of minimum road design standards that may be used in referencing future road construction. The County reserves the right to review all plans for development that may affect the safe and efficient flow of traffic within the County.

Design Element	Primary	Secondary	Residential/Local	Two-Track/Primitive
Right-of-Way Minimum (ft)	60	60	60	40

Minimum recommended design speed	65	60	40	30
Number of lanes	2	2	2	N/A
Lane width (ft)	12	12	12	N/A
Shoulders (ft)	4	2	2	N/A
Maximum Sustained Grade	6.00%	6.00%	6-8%	10.00%
Bridge Width (minimum ft)	32	32	24	N/A
Design Loading	HS20-44	HS20-44	HS20-44	HS20-44
Return Radius	35	30	20-25	20
Typical Speed Limit	65	5	40	30
Cross Slope w/o super elevation %	3(max)	3(max)	3(max)	
Max. Super Elevation	4	4	4	4
Minimum Road Grade %	0.5	0.5	0.5	0.5
Max Grade @ Intersection	2% for 400'	2% for 300'	4% for 150'	4% for 100'
Minimum Pavement Sections Hot Mix Pavement crush gravel	3" 8"	If paved 3" 6"	If paved 2" 6"	N/A N/A

Note: Variations may be approved by the County based on generally accepted engineering practices, references, and standards.

Other design standards such as road horizontal alignment, vertical alignment and clearances, geometric cross sections, side slopes, drainage design, etc. are not delineated here, but adherence to AASHTO standards is implied. All County design standards shall meet the requirements as outlined in the *County Road Fund Manual* as adopted by the Wyoming County Road Standards Committee. Samples of typical road cross-section drawings are included in APPENDIX A.

All new road and bridge construction and any upgrading of the existing roads or bridges commencing after the effective date of this policy shall adhere to these standards unless

administrative relief is granted in accordance with these standards.

Construction of New Roads

Where new roads are proposed to be built by the County or a developer, the proposal shall include an analysis of the projected traffic volumes, information on topography, drainage and extent of cuts and fills, along with construction plans and specifications. The road design and construction specifications shall be reviewed and approved by the County in conjunction with the preliminary plat prior to commencement of construction. The total cost of roads required to serve new development shall be borne by the developer. It shall be the developer's obligation to obtain all necessary rights-of-way, permits, agreements and easements prior to approval of construction plans. Any necessary signage or traffic control devices shall be the responsibility of the developer and follow the specifications stated in Section 3.4.3: Traffic Control and Signage.

Upgrading Existing Roads

Where new development is proposed along existing County roads or proposed to intersect an existing County Road (Driveways/Approaches), the developer's proposal shall include an analysis of the projected traffic volume and impacts, along with information on existing road widths, curves, intersections, and drainage. If requested by the County, a traffic impact study in accordance with accepted traffic engineering standards shall be submitted by the developer. This information shall be reviewed by the County and recommendations shall be made as to what improvements are necessary to accommodate the potential traffic to be generated by the new development.

Occasionally, use or projected use exceeds the potential of an existing road surface. If a proposed development will cause the maximum limits to be exceeded on the adjacent roads, the developer shall improve the affected roads to a classification where the maximum limit is not exceeded. The developer shall be responsible for all costs incurred to upgrade existing County roads unless approved otherwise by the Board.

Roads petitioned to become part of the County Road system, may need to be upgraded to County standards as outlined in this policy before they will be adopted. The determination to bring a substandard road up to County standards shall be made by the Board with recommendation by the County Road & Bridge Superintendent or hired engineering firm.

Bridge Design and Construction Standards

The following are the standards and specifications required for the design and construction of bridges in the County.

- Bridges shall conform to AASHTO, Standard Specifications for Highway Bridges, latest edition.
- The design loading requirements shall conform, at a minimum, to AASHTO HS20-44 specifications.
- Plans and a design report shall be prepared by a Wyoming licensed engineer and shall be submitted to the County for review and approval prior to construction.
- Clear deck width, at a minimum, must accommodate the full width of the travel lanes of approach roads.
- All bridges with a clear span of 20 feet in length or greater shall be submitted to WYDOT's Bridge Department for review and approval before the County approves plans and designs.

Standards for Other Development in County Rights-of-Way

The County recognizes that there are multiple considerations for disturbances in the right-of-way of County roads. The following guidelines are provided to facilitate the design and proper construction. To maintain a reasonable uniform road surface appearance, the County may require a chip seal or other approved remedy over the area of disturbance plus ten (10) feet on either side. Should this be required, and the contractor is unable to chip seal or remediate to County specifications the area due to weather or other causes, the contractor may bond the work or a cost shall be determined by the County and invoiced to the contractor upon completion of the work by the County.

Materials including, but not limited to, cattle guards, culverts, signs, and conduits shall be new unless approved by the County.

Driveways/Approaches

Whenever a property owner, developer, contractor, or other individual proposes to connect a driveway/approach (hereinafter referred to as an "access") to a public roadway, they must obtain a Construction Access Permit from the County prior to commencing construction. A driveway is a private road connecting to a County Road, which serves individual or multiple parcels, residences, commercial businesses, recreation sites, institutional or industrial land uses or a combination thereof. An access way connecting to a County Road which serves a ranch or farm and any associated residence, regardless of length, shall be considered a driveway, and shall meet only such standards as are necessary for public health and safety. The County highly recommends and encourages access roads to be constructed in a manner that allows for emergency vehicle access.

Maintenance of access, including but not limited to any related culverts, drainages, and surfacing, shall be the responsibility of the property owner. The actual location, width and construction requirements are dependent on the use of the access, including circular and/or multiple accesses and will be reviewed and approved by the County during the Permit process. If it is determined that the proposed access may have a significant impact on the County Road, the applicant must adhere to the requirements.

Driveways shall be placed so the following minimum distances are maintained to any road intersection, including a T-intersection on the opposite side of the road from a property where a driveway is proposed.

Minimum distance from intersections: Where the driveway connects to a local access or low volume road, there must be a minimum distance of fifty (50) feet between the driveway's curve return and the nearest curve return of the road being intersected. Where a driveway connects to a collector or larger road, there must be a minimum distance of one hundred (100) feet between the driveway's curve re- turn and the nearest curve return of the road being intersected.

Spacing between driveways: Driveway openings shall be not closer together than a minimum of forty (40) feet as measured from curve return to curve return, or else shall be combined. More spacing may be required for traffic safety and proper traffic operation.

Shared driveways: Developers or property owners proposing the use of shared driveways shall record an easement defining the location of the driveway and who is to benefit. Proof of easement is required before a Right-of-Way / Access Permit for a shared driveway will be granted.

Driveway widths: The dimensions of driveway widths and radii shall be as shown in Table below: Driveway Surface Widths. Proposed driveway widths shall be provided on Right-of-Way / Access Permit Applications to be approved by the County prior to construction.

Type of Service	Minimum/Maximum Driveway	Maximum Return Radius
Commercial/Other	20 ft. Min / 40 ft. Max	30 feet
Field Entrance	20 ft. Min / 24 ft Max	15 feet
Individual Residence	20 ft. Min / 24 ft Max	20 feet
Duplex of Multiple Residences	20 ft. Min / 40 ft Max	20 feet
Multi-family	20 ft. Min / 40 ft Max	20 feet

- To be determined at time of site plan review

Grades: Driveways shall have a maximum grade of eight (8) percent and shall provide a reasonable transition in terms of grade between the edge of the County Road and at least the initial 25 feet of the driveway length. For single family residences and duplexes located on lots having difficult terrain, driveway grades may exceed eight (8) percent as approved by the County.

Vehicle turnarounds: All driveways connecting to roadways with daily counts greater than five hundred (500) ADT shall include an area outside of the County Road right-of-way allowing vehicles to safely turn around to avoid having to back onto the roadway when exiting.

Surfacing of driveways: Driveways may be either graveled or paved. Where a driveway is to be graveled, the surface shall be constructed of minimum of three (3) inches of crushed road base compacted to ninety-five (95) percent standard proctor. Where a driveway is to be paved, the surface shall be constructed with a minimum of six (6) inches of crushed road base compacted to ninety-five (95) percent covered with a minimum of two (2) inches of asphalt.

Provisions for drainage: Driveway design shall make adequate provision for drainage and prevention of erosion. All driveways shall have minimum fifteen (15) inch diameter culverts to handle roadside drainage, unless otherwise approved by the County.

Minimum sight distance: The first ten (10) feet of the driveway from where it intersects the County Road shall be designed and located to provide a minimum sight distance clear of all obstructions, natural or man-made, for at least two hundred (200) feet in either direction on local access roads or four hundred (400) feet on collector roads.

Perpendicular approach to County Road: Accesses shall be constructed perpendicularly to the County Road. If perpendicular access is not feasible, then a request for a waiver or variance of the perpendicular requirement shall be submitted in writing to the County for review and approval.

*See Typical access location drawing for additional information

Parking Area and Turnouts

Whenever a property owner, developer, contractor, or other individual proposes to connect a parking area or turnout to a public roadway, they must obtain a Right-of-Way Permit from the County prior to commencing construction per Section 3.6.4: Right-of-Way Permits.

The following are the standards for parking areas and turnouts adjacent to County Road rights-of-way.

1. Minimum distance from intersections: Where a parking area or turnout connects to a local access or low volume road, there must be a minimum distance of fifty (50) feet between the parking area or turnout's curve return and the nearest curve return of the road being intersected. Where a parking area or turnout connects to a collector or larger road, there must be a minimum distance of one hundred (100) feet between the parking area or turnout's curve return and the nearest curve return of the road being intersected.
2. Minimum Radii: Parking areas and turnouts shall have a minimum radius of thirty (30) feet.
3. Grade: Parking areas shall have a maximum grade of four (4) percent and a minimum grade of (1) percent to facilitate drainage away from the County roadway.
4. Surfacing: All parking areas shall be constructed with a minimum of six (6) inches of crushed road base compacted to ninety-five (95) percent standard proctor. Paving of a minimum of two (2) inches of asphalt will generally be required when fifteen (15) or more parking spots are established for commercial or industrial facilities. Due to the frequency of use and specific function, the design and construction of parking lots comprised of fifteen (15) or more parking spaces must be approved by the County.
5. Provisions for drainage: Parking area and turnouts design shall make adequate provision for drainage and prevention of erosion. All parking areas and turnouts shall have minimum fifteen (15) inch diameter culverts to handle roadside drainage, unless otherwise approved by the County.
6. Minimum sight distance: The first ten (10) feet of the parking area or turnout from where it intersects the County Road shall be designed and located to provide a minimum sight distance clear of all obstructions, natural or man-made, for at least two hundred (200) feet in either direction on local access roads and four hundred (400) feet on collector roads.

Traffic Control and Signage

The County endeavors to maintain safe traffic control throughout the County. All signs, striping, markers, delineators, signals, and other traffic control devices shall conform to the requirements

of the Manual on Uniform Traffic Control Devices {MUTCD} latest edition, published by the U.S. Department of Transportation, Federal Highway Administration. The County will continue to update signs as needed to remain current with the standards set by the MUTCD.

In new developments, all required street sign names, speed limit signs, stop signs and other traffic control devices shall be paid for and installed by the developer and approved by the County. Nonstandard signs or other traffic control devices are subject to rigid control and approval by the County shall be obtained prior to their use. All signing and striping plans shall be submitted to the County for approval.

The following are additional considerations related to common traffic control and signage in the County.

1. **Signs with County Rights-of Way:** All signs within County rights-of-way are posted solely at the County's discretion and in compliance with the MUTCD, latest edition. Individuals or others re-questing posting of any signage within a county right-of-way must make a formal request in writing to the County Road and Bridge Superintendent. The County Superintendent will make a recommendation to the Board, who will make the final decision to approve or disapprove. If permission is granted for posting, all material expenses will be borne by those requesting same and the maintenance thereof; installation of private road signs within County rights-of-way will be at the direction of the County.
2. **Address Markers:** New and replacement address markers purchased by landowners, or the County for County-assigned physical addresses will be ordered and installed by the County Road and Bridge Department. Refer to APPENDIX C: for address marker specifications.
3. **County Road Signs:** See below for County Road sign specifications.
4. **Subdivision and Private Road Signs:** Signs within subdivisions or along private roads are the responsibility of and are to be maintained by the subdivision developer and/or landowner(s). Subdivision and private road signs which are in a county right-of-way must conform to the standards.

Mailboxes

All new mailbox installations within a County right-of-way shall require the approval of the U.S. Postal Service and the County.

- Mailboxes shall have turnouts constructed with joint consideration of the box holder, the County, and the U.S. Postal Service.
- All mailboxes and turnouts must be designed, constructed, and installed in conformance with Parking Areas and Turnouts.
- Mailboxes shall be stationed at least 10feet from the edge of the traveled roadway to move the box holder and the mail carrier safely out of the lane of traffic; likewise, snow plowing, borrow ditch maintenance and right-of-way mowing operations are considerably easier for the County maintenance crews.
- Box holders may remove or replace their existing mailboxes and stands at their own expense, provided the design of their structure is not deemed a safety hazard by the U.S. Postal Service or the County.

- When the County has a road reconstruction, repair or upgrade project affecting mailbox stationing, the Road and Bridge Foreman will be responsible for turnout construction, notification of box holders and scheduling of construction.

If a mailbox is damaged by the impact of plowed snow or ice, it shall be replaced or repaired by the mailbox owner and at the owner's expense.

If a mailbox or the mounting post is accidentally struck or damaged by direct contact by a County snowplow or other County-operated maintenance equipment, it shall be repaired or replaced by the County with an equal size and quality mailbox.

The replacement by the county will only be done if the damaged mailbox was installed and positioned in accordance with the standards identified above.

Cattle Guards

The County maintains existing cattle guards on the County Road system. If a property owner requests a new cattle guard within the County Road rights-of-way system, the Board, per W.S. §11-28-105, shall consider the request and recommendations of the County Road and Bridge Foreman and may authorize the installation. The Board of County Commissioners shall be responsible for determining if the petitioner or County shall be responsible for payment. If a cattle guard is installed at the request of a property owner, the property owner shall reimburse the County for the cattle guard materials. Installation of the cattle guard will be completed by the County. The property owner is responsible for all fencing to be connected to the new cattle guard in accordance with state statute regarding fences and cattle guards. All cattle guard installations require a Right-of-Way Permit and must be designed, constructed, and executed in conformance with the standards defined in this policy.

Cattle Guards Installed on Graveled Roads

Following are the standards for cattle guards installed on graveled roads in the County.

1. The cattle guard shall be in accordance with the requirements of Federal Highway Administration Standard Specifications for construction of Roads and Bridges on Federal Highway Projects (FP-03).
2. Sections of cattle guard shall be of sufficient number and length to reach the total width of the designed road top.
3. Foundation supports (blocks) must be concrete and split at the center of the road to allow 2% crown. Concrete blocks should have belting or a similar type of material for the steel grate of the cattle guard to sit on.
4. Guard wings, or other construction to allow attachment of partition fence to cattle guard, should be constructed of materials comparable to that of cattle guard.

Cattle Guards Installed on Paved Roads

Cattle guards installed in paved road rights-of-way shall be in accordance with WYDOT Standard Plan and Specifications for cattle guards. Sloped wing or swing-wing guards may be used. Backfill material for the installed cattle guard shall be accomplished by placing in 8" lifts and compacting each lift to 95% density of standard proctor for each soil type used. Surfacing on both sides, the full width of the installed cattle guard, shall be patched with a minimum of 6" of Grading W

Crushed Base compacted to 95% of maximum density topped with 3" of Commercial Hot Plant Mix Pavement compacted to 92% of maximum density determined by the mix design.

Fences

The County does not install or maintain fences along its roads or rights-of-way. Should a property owner wish to install a fence adjacent to a county right-of-way, the fence must be installed in accordance with the following standards. Maintenance of the fence shall be the responsibility of the property owner.

- Generally, fences are to be installed beyond the County right-of-way. If a property owner desires a fence to be installed within the County rights-of-way system, the property owner shall make a written request to the Road and Bridge Superintendent. Upon review of the request, the County Superintendent will make a recommendation to the Board. As per W.S. §11-28-105, the Board shall consider the request and recommendations and may authorize the construction. All costs associated with such requests shall be borne by the property owner making the request.
- All fences within or along County rights-of-way shall comply with Wyoming State Statutes (see W.S. §11-28-101 through 108).
- All fences within or along County rights-of-way shall be designed, constructed, and installed in conformance with best practices and all applicable sections of this policy.

Utilities

The County recognizes the need and common practice for public utilities to be placed within the County right-of-way. When improperly installed, these utilities may create hazards or be detrimental to the purpose and function of the County Road system. It is the County's preference that utility providers first try to obtain right-of-way outside of a county right-of-way. If that is not feasible, then it is the intention of the County to locate these utilities in accordance with the following standards.

General

All utilities to be placed within County rights-of-way shall submit a Right-of-Way Permit Application to the county in accordance with this policy and the application instructions. A Right-of-Way Permit must be granted by the County before construction may commence. Any infrastructure and work related to its installation within any County right-of-way associated with a Right-of-Way Permit shall be, regardless of who performs the construction, the responsibility of the utility provider.

Following are general considerations for all types of utilities:

- **Positioning:** The County reserves the right to determine the positioning of utilities proposed to be placed in the County right-of-way. All utilities crossing the right-of-way shall do so as perpendicular to the centerline of the county road as practical.
- **Safety:** All excavations made by the utility provider in the roads shall be properly safeguarded for the prevention of accidents.
- **Emergency Repairs:** In the event that emergency repairs which impact the flow of traffic or modify the construction that was originally permitted are necessary, the utility provider shall immediately notify the County of the need for such repairs. A utility provider may initiate such emergency repairs but must apply for appropriate permits

within the first two business days after discovery of the emergency. The utility provider shall comply with all applicable County regulations relating to such repairs, including the payment of permit or license fees.

- **Relocation:** If a utility is found to conflict with work proposed by the County in a County right-of-way, the utility provider may be required to relocate its utilities. If it is determined that relocation is necessary, the expense thereof shall be paid by the utility provider. If the County plans any projects that may require a utility provider to protect, support, temporarily disconnect, relocate, or remove any of the utility provider's property, then the County shall promptly notify the utility provider of the extent and likelihood of any such projects. Upon notice, the utility provider shall, at a time specified by the County, protect, support, temporarily disconnect, relocate, or remove any of its property to allow the County to perform its work.

Underground Utilities

Installation of buried utilities must be done in accordance with all laws, codes, specifications, and best practices associated with that utility. All a utility provider's work under this section shall be done in strict compliance with the latest edition of the WPWSS. The permittee must comply with the provisions of §37-12-301 through §37-12-304 titled "Damage to Underground Public Utilities Facilities." Installation of buried utilities shall not interfere with existing utilities within County rights-of-way. New utility installations shall be accomplished to ensure that existing utilities may perform maintenance work on their facilities with minimal interference from the newly installed utilities. Refer to APPENDIX E: Typical Trench Cross Section Detail for information on installation of underground utilities.

Following are general considerations for underground utilities:

- **Location:** The location of a buried utility, when running longitudinal within the right-of-way, shall be at the discretion of the Road and Bridge Superintendent; however, it shall not be placed under the surface of the road-way. The Superintendent may specifically approve a buried utility to be located under the surface of the roadway if they find it to be in the best interest of the County. The County discourages road cuts for utilities and encourages boring for installation. If pavement is being cut, a cut fee as well as a bond will be required as outlined on the Right-of-Way Permit.
 - It is the responsibility of the utility provider to provide the County and the public with locates of their utility line upon request. All non-metallic facilities shall have a tracer wire installed with the utility to support utility locates. The minimum tracer wire shall be #12 AWG solid copper.
- **As-Built Drawings:** Upon completing the installation of a buried utility in the county right-of-way, a copy of the "as-built" drawings shall be supplied to the County. The drawings shall be at a reasonable scale to show the location of the buried utility within the County right-of-way, the depth of bury and all other information necessary for the county to have reasonable knowledge of the facility within the right-of-way. These drawings may be submitted in an electronic format acceptable to the County.
- **Excavations:** Utility providers may wish to make excavations in the right-of-way for the maintenance or extension of the utility system. Prior to doing such work, the

utility provider shall apply for and obtain appropriate permits from the County. The utility provider must also give appropriate notice to any other franchisees, licensees, or permittees of the County or other units of government owning or maintaining pipes, wires, conduits, or other facilities which may be affected by the proposed excavation.

Whenever a utility provider disturbs the surface of any County Road for any purpose, the permittee shall promptly restore the road to at least the prior condition or the required minimum standard, whichever is better, and to the satisfaction of the road foreman. The utility provider shall be responsible for the maintenance of the road and its surface within the area affected by the excavation for the duration of the disturbance. The utility provider shall guarantee the restoration of the road surface for at least one year against defective materials and workmanship. If the County finds the restoration work to be inadequate as determined by the foreman, notice will be sent to the utility provider. If the excavation site is not brought up to standards, the County may refill and/or repave the site at the expense of the utility provider. Costs include, but are not limited to, the cost of inspection and supervision.

Above Ground Utilities

Installation of aboveground utilities must be done in accordance with all laws, codes, specifications, and best practices associated with that utility. Any work operation in the vicinity of electric power lines shall be in compliance with §37-3-301 through §37-3-306 titled "Wyoming High Voltage Power Lines and Safety Restrictions Act."

Following are considerations for the installation of aboveground utilities:

- All aboveground utilities shall be located and installed so as not to cause unnecessary obstruction to pedestrian and vehicular traffic or damage to the utility itself.
- No pole or structure above ground shall be placed within a pedestrian walkway nor set closer than twelve (12) feet to the shoulder of any County Road; a lesser distance, however, may be allowed if in-sufficiently cleared right-of-way is available to meet this minimum distance. In no case will a pole, guy wire and/or anchor be permitted within twelve (12) feet of the shoulder of a County Road except light and traffic control poles with breakaway bases.
- Overhead lines shall be permitted to sag under worst anticipated conditions no lower than eighteen (18) feet above the roadway and shall preferably be no closer than twenty-three (23) feet.

Liability

- The County assumes no responsibility for damage to utilities. All utilities shall be adequately marked or properly buried within county rights-of-way.
- Any contractor or utility provider installing a utility in a county right-of-way that is not in accordance with approved plans or permits will be subject to a penalty and bear all costs associated with proper relocation of that utility.
- At the County's sole discretion, any utility not properly marked, buried, or installed according to its approved plan or permit may be terminated by the county and the utility will be required to be removed.

Culvert Design

The County maintains culverts within the County Road system to direct drainage for the protection of the roadway. The County recognizes that the installation of culverts to direct water for irrigation and drainage purposes is common.

Following are standards for culverts crossing a County Road:

- Culverts for roads that have an oil surface shall be sized to pass the anticipated 1% annual chance flood volume capacity. Culverts for roads that have a gravel surface shall be sized to pass the anticipated 2% annual chance flood volume capacity.
- Culverts to be placed under any road shall be no smaller than 18 inches in diameter; culverts smaller than 18 inches are easily blocked by debris.
- The minimum length of any culvert in a public right-of-way will be the length necessary for the top of the pipe to bisect the slope of the ground on each side of the crossing plus two (2) feet. Irrigation culverts shall extend from right-of-way to right-of-way.
- The minimum bedding material over the culvert should be 12 inches, compacted.

All other pertinent installation procedures should meet the standards in the latest edition of the Wyoming Public Works Standard Specifications. Further design standards can be found in the AASHTO Roadside Design Guide and other applicable standard setting publications.

Irrigation Facilities

The County recognizes the need for and importance of irrigation facilities; however, these facilities may create hazards or be detrimental to the purpose and function of the County Road system. It is therefore the practice of the County to locate irrigation facilities outside of County rights-of-way whenever possible. Though natural flood events may occasionally present problems of water dammed against a county road, the County tries to provide effective drainage of all water away from the roadbed. Irrigation and wastewater will not be allowed to collect against a county road and waste ditches within the right-of-way are not allowed unless approved in writing by the Board.

Irrigation facilities which must be within the County rights-of-way shall be constructed to reduce impacts to the road system and eliminate hazards to the traveling public and require a Right-of-Way Permit. At a minimum, the following practices shall not be allowed within the County rights-of-way:

1. New or reconstructed supply and waste ditches.
2. New or reconstructed diversion structures.
3. New or reconstructed open or unlined channels; and
4. Sprinkler systems that spray into county rights-of-way.

The County will attempt to work cooperatively with irrigation districts and irrigators to resolve conflicts with new and existing facilities. Those districts or irrigators who willfully continue to violate this section, will be subject to legal action.

Guardrail

Guardrail is not commonly used in the County Road system because it is considered an obstruction. However, the County recognizes there are permanent obstructions of greater risk, therefore guardrail is installed to mitigate the potential risk to the traveling public where warranted. Guard railing should be installed as budgets allow and safety circumstances warrant new installations. Guardrail should be installed to AASHTO design standards or as approved by the Board.

MAINTENANCE

Asphalt Road Treatments

It is the responsibility of the Road & Bridge Department to maintain all existing asphalt roads to design standards as time and budgets allow. In recent years, rising costs of materials have created a strain on local budgets to maintain existing asphalt roads within the County. In order to keep the infrastructure in good condition, the County will conduct regular inspections of the roadway surfaces. Through visual observation and sound professional judgment, determination will be made as to which roads require treatments and which types of treatments are necessary. Annual road patching and crack sealing should be standard practice, with larger areas addressed using cold or hot mix, grader, and roller to help maintain the integrity of the road. Other treatments may include slurry seals, fog seals or other chemical treatments that would be economically feasible. Overlays with either hot mix or cold mix will ultimately be necessary, but other treatments should be used extensively prior to that decision.

As an accepted practice, chip seal projects will be used to keep asphalt roads in good to excellent condition, typically on a seven-year rotation. As roads deteriorate, sound engineering judgment and availability of funds will be considered when identifying and prioritizing reconstruction projects. Decision-making criteria include traffic counts, safety conditions for the public, primary users, connectivity, etc. To supplement the County's budgeted road funds, additional sources of funding should be pursued such as Federal grants and cost share programs, WYDOT cost share programs, and industry cooperation to help meet those road conditions caused by traffic specific to certain industry. It may become necessary to ask the local taxpayers to entertain a special mill levy applied only to roads, as has been used in other counties.

Graveled Roads

The majority of the County's roads have graveled surfaces. These types of roads are less expensive to maintain and offer a variety of options to keep them in good condition. Of utmost importance to any road design is proper drainage. Adequate crown of the road (2% is the accepted standard) and enough roadbed elevation will allow water to get off the roadway. Clean borrow ditches and clean cross-piping to prevent water from accumulating along roadways, coupled with water bars and drainage ditches to get water away from roads, will keep roads from subbing and ultimately failing.

Studies have proven that properly designed gravel mixes lend themselves to better ride-ability, less wash-boarding and better acceptance of chemical treatments for dust abatement such as magnesium chloride, calcium chloride, lignin sultanaates or similar type treatments. These

treatments may be considered on a case-by-case basis as the need arises and the budget allows within the county.

Roads should be maintained with motor-graders, followed by a roller, making the most of the available moisture in the road. Shoulders should be pulled in on a regular basis to help maintain the proper profile and width of the road. As the need arises, additional gravel should be added to keep the integrity of the road maintained.

Revegetation

Whenever roadway or bridge construction results in earth disturbance, revegetation is required and shall be completed in the spring or fall after construction. Native or similar horticultural material shall be used. All areas disturbed by construction operations not otherwise covered by structures or pavement must be seeded, fertilized, mulched, planted, and otherwise treated to provide an established stand of vegetation in accordance with the WPWSS. Cut and fill slopes must be treated to prevent erosion. Areas not disturbed by construction shall be left in their present vegetative state, except where the thinning of trees may be required.

Weeds and Other Invasive Species

The County is conscious of the threat of weeds and other invasive species to its rights-of-way and other properties affecting adjoining individuals and landowners. The County has used and will continue to use mechanical and chemical means to control all noxious weeds on County properties and rights-of-way. This is accomplished by in-house methods and in cooperation with Carbon County Weed and Pest resources.

Bridge Maintenance

Road and Bridge will monitor bridge conditions, particularly during flood events, to identify signs of structural deficiencies to promote the safe travel of the public across bridges on County rights-of-way. Repairs or replacements will be determined following physical examination by maintenance crews or by recommendations from WYDOT inspectors.

The County participates in the WYDOT Bridge Inspection Program in which bridges in the County Road system are inspected by WYDOT every two years to maintain a safe and efficient bridge system. The County will continue to review inspection reports and perform required maintenance in a timely manner.

Snow Removal

It is the County's practice to begin snow plowing operations after an accumulation of at least six (6) inches of snow. Any plowing that occurs prior to that amount of accumulation is done as a courtesy to the public. The priority of routes to be plowed is governed by school bus and mail routes having first priority, followed by all other roads holding second priority. There are special cases where clearing roads to critical facilities takes precedence. The Road and Bridge Supervisors have sole discretion in mobilizing the County's resources for snow removal. Please see the attached snow removal policy below.

Due to the lack of delineators or striping on county roads, all plowing is provided only during

daylight hours. During a snow event, if a Road and Bridge Foreman deems it unsafe for crews to be on roadways, operations will be discontinued until conditions allow for safe removal of snow accumulation. Exceptions may be allowed for emergencies. Please see the attached snow removal policy below.

Snow Removal Policy

Our policy is to keep the roads open, safe, and usable for all drivers, to the extent that we possibly can. The following plan covers the procedures Carbon County Road & Bridge will follow during the winter months for snow removal. Any of these operations may be altered at the discretion of the Road & Bridge Department depending on conditions and/or available resources.

Our priorities are the more highly traveled roads and the main arteries, such as established school bus routes and mail routes. We start early in the mornings with fresh snowfall and try to get as many roads plowed as early as possible. On most days we plow snow all through the day. We are not a 24/7 establishment, and we cannot work around the clock. The Road & Bridge Department will collaborate with each District Area to determine whether snow removal operations are necessary outside the normal scheduled work hours. The normal schedule work week is Monday through Friday, 6:30 AM – 3:00 PM.

When there is significant snowfall and high winds develop, we are not able, at times, to keep all the roads open. There are situations where, in just 10 or 15 minutes, the road goes from freshly plowed to impassable again. In such instances, the decision may be made by the Road & Bridge Department to suspend all plowing operations, until the wind/storm diminishes, and actual progress can again be realized in keeping the roads open. Even when that happens, we still work with the school transportation department to ensure the buses are cared for and the kids are returned home safely.

We do not respond to calls from people who become stuck and want help getting out, or who want a ride to their destination. We are not a towing company, or a public transportation company. You are responsible for knowing the weather conditions that may impact these county roads. You must use your judgment on the weather conditions and whether you and the vehicle you're in possess the skills to traverse the road. With that in mind, the decision and responsibility for winter travel falls to the individual. Our purpose is and must be to open the roads in a safe manner.

We often receive complaints from those living along county roads who say that our clearing the drifts from the road results in their driveway approaches being filled with snow. Unfortunately, that does happen, and when using a V-plow or trying to move as much snow as possible downwind to keep the roads open, the result may be snow in driveways. If we were to stop and plow out every driveway along every county road, it would take us several days longer to cover the whole county with the plows. That makes it unfeasible to do so. The other option would be to not plow the section of road in front of their driveway, but then they would still be blocked in. So, we must deal with several hundred miles of drifted and blocked county roads, and they may have to deal with the 3 or 4 feet at the end of their driveway. We cannot go outside of our county right of way or across pipelines to make a winter road. Bottom line, the county will not plow private drives or roads of any kind.

We know there are a lot of people who absolutely must use county roads for very good reasons, such as cattle-feeding operations, and those with other livestock to care for. In many cases those people do not wait for us to get there when we can, but rather take it on their own to use their loaders, tractors, and so on to open the roads they need to use. We would just ask that they coordinate with us beforehand and try not to tear up the road or remove the gravel along with the snow. However, if private snowplowing on a county road occurs on a regular basis, please contact Road & Bridge Department to request a snow removal permit.

On occasion, too, there are legitimate emergency situations where access to a county road is a must, such as those who become lost or have various medical needs. The Road and Bridge is committed to doing all we can to assist in such critical matters, whenever they arise. We would

encourage people in the area, especially those considerable distances from town, or those who live on less traveled county or state roads, to give thought to keeping on hand adequate supplies of food, medicine and prescriptions, propane, livestock feed, and other daily essentials. It may be prudent to stock up on such items at times when the weather and the road conditions are good for travel.

Finally, when Emergency Management issues a travel advisory, or asks people to eliminate unnecessary travel and to just stay home, that advice should be heeded carefully. The Road and Bridge Department will work together with local law enforcement and other agencies to keep the public advised of current conditions and at times when the plowing is suspended until conditions improve, the public will be made aware of it. People need to weigh carefully whether driving in such conditions is necessary, and if it is worth the risk.

If you have an emergency call 911 immediately

NO SEASONAL ROUTES WILL BE PLOWED

List of Seasonal Roads

Baggs Area

County Road 501	Cherry Grove Road	No Winter Maintenance from Milepost 4.3 to 8.6
County Road 503	McCarty Canyon	No Winter Maintenance from Milepost 1.0 to 37.2
County Road 602	Browns Hill Road	No Winter Maintenance from Milepost 0.0 to 17.0
County Road 605S	Twenty Mile Road South	No Winter Maintenance from Milepost 0.0 to 1.5
County Road 607	Dolan Lane Road	No Winter Maintenance from Milepost 1.2 to 2.0
County Road 608	Wild Cow Road	No Winter Maintenance from Milepost 0.0 to 22.5
County Road 706	Big Gulch Road	No Winter Maintenance from Milepost 2.0 to 6.0
County Road 730N	Paintbrush Road	No Winter Maintenance from Milepost 0.0 to 2.15
County Road 752	Stock Drive Road	No Winter Maintenance from Milepost 0.0 to 5.0
County Road 754	Sandstone Stock Drive	No Winter Maintenance from Milepost 0.1 to 3.2

Encampment Area

County Road 211	Black Hall Road	No Winter Maintenance from Milepost 4.1 to 6.6
County Road 353	Finley Hill Road	No Winter Maintenance from Milepost 1.0 to 6.6
County Road 387	South Spring Creek Road	No Winter Maintenance from Milepost 12.01 to 16.5
County Road 798	Holroyd Road	No Winter Maintenance from Milepost 0.0 to 4.3
County Road 660	Sanger French Creek Road	Minimal Maintenance from Milepost 10.21 to 14.33

Rawlins Area

County Road 100	Ferris Road	No Winter Maintenance from Milepost 0.0 to 14.2
County Road 345	Savage Ranch Spur	No Winter Maintenance from Milepost 0.0 to 5.1
County Road 347S	Savage Ranch Spur South	No Winter Maintenance from Milepost 0.0 to 7.1
County Road 401	Sage Creek Road	No Winter Maintenance from Milepost 5.7 to 45.5
County Road 497	Buzzard/Willow Hill Road	No Winter Maintenance from Milepost 0.0 to 42.1
County Road 499	North Ferris Road	No Winter Maintenance from Milepost 4.3 to 15.0
County Road 503	McCarty Canyon Road	No Winter Maintenance from Milepost 0.0 to 36.2
County Road 505E	Miller Hill Road East	No Winter Maintenance from Milepost 0.0 to 7.0
County Road 505W	Miller Hill Road West	No Winter Maintenance from Milepost 0.0 to 18.4
County Road 605N	Twenty Mile Road North	No Winter Maintenance from Milepost 2.0 to 24.35

Saratoga Area

County Road 215S	Stock Driveway Road South	No Winter Maintenance from Milepost 0.9 to 7.8
County Road 385	North Spring Creek Road	No Winter Maintenance from Milepost 10.25 to 15.9
County Road 400	Rattle Snake Road	Minimal Maintenance from Milepost 3.26 to 11.4
County Road 406	ZN Road	No Winter Maintenance from Milepost 0.0 to 3.19

County Road 408	Bolton/4 Bar Lane Road	No Winter Maintenance from Milepost 3.45 to 10.25
County Road 500	Jack Creek Road	No Winter Maintenance from Milepost 11.1 to 23.63
County Road 504	Hay Creek Road	No Winter Maintenance from Milepost 11.2 to 15.3
County Road 508	Pick Bridge Road	No Winter Maintenance from Milepost 3.02 to 6.91

Hanna Area

County Road 102	Leo-Shirley Basin Road	No Winter Maintenance from Milepost 0.0 to 21.7
County Road 191	Troublesome Road	No Winter Maintenance from Milepost 0.0 to 3.0
County Road 316	Old Lincoln Highway Road	No Winter Maintenance from Milepost 2.0 to 6.9

Elk Mountain Area

County Road 15	Cooper Cove Road	No Winter Maintenance from Milepost 1.2 to 11.4
County Road 101	Medicine Bow Ranger Station	No Winter Maintenance from Milepost 6.0 to 11.7
County Road 400	Rattle Snake Pass	No Winter Maintenance from Milepost 11.4 to 20.3
County Road 402	Elk Mountain Arlington Road	No Winter Maintenance from Milepost 3.0 to 13.8

Medicine Bow Area

County Road 1	Medicine Bow – McFadden	No Winter Maintenance from Milepost 10.0 to 20.48
County Road 2W	Shirley Ridge Road West	No Winter Maintenance from Milepost 0.0 to 7.9
County Road 97	Shirley Road	No Winter Maintenance from Milepost 4.4 to 8.0
County Road 99	Little Medicine Road	No Winter Maintenance from Milepost 0.0 to 14.0
County Road 102	Leo-Shirley Basin Road	No Winter Maintenance from Milepost 0.0 to 20.0
County Road 103	Dry Creek Road	No Winter Maintenance from Milepost 0.0 to 9.5
County Road 115	Elk Mountain/Carbon Road	No Winter Maintenance from Milepost 1.0 to 14.6

Carbon County Road & Bridge Main Office
PO Box 487
Rawlins, WY 82301
307-324-9555

Carbon County Sheriff's Office 307-324-2776

Baggs Shop – 307-383-6432
Encampment Shop – 307-327-5767
Elk Mountain Shop – 307-348-7392
Saratoga Shop – 307-326-5681
Medicine Bow Shop – 307-379-2286
Hanna Shop – 307-325-6820

List of Carbon County Roads that are access only road with NO Maintenance to very limited maintenance from Carbon County Road & Bridge – meaning these roads should remain county roads for access to BLM and state land only.

Access Only Road – Category 4 Roads (102.31 Miles)

County Road 2W – Shirley Ridge West – All of it 7.8 miles – Medicine Bow District
County Road 75 – Cheesbrough Hill/Richards Lane – all of it 1 mile – Medicine Bow District
County Road 103 – Dry Creek – 9.5 Miles – Medicine Bow District
County Road 115 – Elk Mountain/Carbon – From Cemetery to Arch Mineral Access on Elk Mountain Side – 10 miles will be access only – Medicine Bow and Elk Mountain District
County Road 191 – Troublesome – 1.5 miles – Medicine Bow District
County Road 272 – Stone Ranch Road – 1.1 miles – Rawlins District
County Road 316 – Old Lincoln Highway – 4.9 miles – Hanna District
County Road 305 – Castle Basin Road – 3.5 miles – Rawlins District
County Road 345 – Savage Ranch Spur – 5.1 miles – Rawlins District
County Road 347S – Savage Ranch South – 7.1 miles – Rawlins District
County Road 402 – Elk Mountain/Arlington Road – All of it 13.3 Access Only limited maintenance - Elk Mountain District
County Road 408 – Bolton Road – From Junction of 408/508 MP 7.04 to end of road at locked gate at MP 10.25 – Total 3.21 miles – Saratoga District

County Road 412 W – Oberg Pass – 3 miles – Agreement in file as access only road when 412 was abandoned years ago Saratoga/Elk Mountain District

County Road 440 – McFadden/Rock River – 2.3 miles – Elk Mountain District

County Road 488 – Energy Road – 1 mile – Elk Mountain District

County Road 497 – Buzzard Road – From Stone Ranch Approach at MP 12 to back side of Buzzard Ranch at MP 30 all access only through that area – 18 miles – Rawlins District

County Road 499 – North Ferris Road – From Green gate at MP 4.6 to Buzzard Road at MP 15 will all be access only – 10 miles – Rawlins District –

Inventory of County Roads

The County, through the Road and Bridge Superintendent, shall maintain an inventory of County roads. All county road records shall be kept by the Road and Bridge under the direction of the County Clerk.

Use of County Equipment, Materials and Supplies

It is expressly understood that all County Road and Bridge equipment is for use only by qualified employees of the County for County projects. This rule is to comply with insurance and liability issues, as well as permitting requirements. Likewise, County Road building materials and supplies are for the express use of the County and should not be sold to the public for private use. Exceptions may be granted for emergency use if an agreement is reached as to the replacement of used materials or supplies.

Reciprocal agreements and use of County equipment, materials and supplies may occur from time to time between other city, county or state governments as may be encountered in day-to-day work. These agreements or use are generally for the economic advantage of taxpayers and to provide more efficient use of public assets.

Encroachment in Rights-Of-Way

The County has sole authority for maintenance and regulation within established County rights-of-way. Encroachment by the public, utilities or other governmental agencies is prohibited except by written agreement or permitting as established by this policy. Exceptions, granted by the Board, may be allowed on a case-by-case basis, but are generally discouraged.

Examples of encroachment may include but are not limited to:

- Vehicles or equipment parked within County rights-of-way are deemed a safety hazard.
- Obstacles that cause limitations to plowing or maintenance of roads.
- Buildings or other structures within rights-of-way that could pose a safety issue or sight impediment.
- Illegal signage.
- Unapproved fencing, corrals, panels, power poles, utility, and garbage containers.
- Private septic systems.
- New irrigation structures; or
- Any other obstruction that is deemed unsafe for the movement of traffic.
- Those with an encroachment violation may be notified either verbally or in writing by a Road and Bridge Foreman, the County Superintendent, the County Attorney, or a Law Enforcement Officer. If the encroachment is not addressed and safely removed according to the terms specified in the notice, a fine may be levied against the violator.

Right-of-Way Permits

Prior to the commencement of any construction or encroachment within a County right-of-way, a Right-of-Way Permit shall be obtained. A Right-of-Way Permit is required for the following activities:

- Driveways and Approaches
- Parking Areas and Turnouts
- Utility Installations
- Culverts or other drainage structures
- Cattle guards
- Mailboxes
- Fences
- Signage
- Irrigation facilities
- Landscaping
- Other items that might interfere with safe passage of traffic.

Right-of-Way Permit Applications are available by calling the Road & Bridge Department (307) 324-9555 – emailing kandisfritz@carbonwy.com or stopping by (1016 West Hugus, Rawlins, WY 82301) or on the County website: www.carbonwy.com

Right-of-Way Permits must be in the name of the entity responsible for the installation and maintenance of the proposed encroachment. Right-of-Way Permits are required to assure the method of installation meets the specifications in this policy, provides for the safety of the public, follows generally accepted engineering practices, and takes into consideration adequate revegetation of disturbed areas. These permits are also intended to assure adequate reconstruction and/or repair of any damage caused to County roads or road rights-of-way. The permittee shall be responsible for a period of one (1) year after final inspection of work for any maintenance or repair necessary to keep the roadway in an acceptable condition.

County projects are exempt from these permit requirements but are not exempt from complying with this policy.

Oversize Load Policy and Permits

Road and Bridge has a very detailed policy for Oversize and Overweight loads along our county roads. Please go to the county website under Road and Bridge to get the policy and required permits. www.carbonwy.com

Permit Fees and Performance Bond Requirement

Permit fees apply for select types of projects; each being noted on the Right-of-Way Permit Application. Fees for Right-of-Way Permits are set by separate resolution and reviewed on an annual basis.

Before issuance of a Right-of-Way Permit, the County may require the applicant to provide proof of insurance and to post a performance bond, in the form of a letter of credit, cashier's check or other approved form from a licensed surety, in an amount sufficient to complete the project or restore the construction area. All insurance and bonding shall be done to the satisfaction of the County prior to issuance of the permit. The bond amount shall be specified on the approved permit. All bonds must have an expiration date equal to or greater than one year after the anticipated date of final inspection.

Criteria to be used in determining whether a performance bond will be required may include, but is not limited to:

1. Estimated cost of the project. If the project's estimated costs are less than seven hundred fifty dollars (\$750), generally a performance bond will not be required.
2. Type of disturbance. If the work involves cutting or disturbing a paved roadway, a performance bond will be required.
3. Scale of the disturbance; and
4. Experience with, or prior knowledge of, the contractor/developer.

If the work and installation are not completed as stated in the approved permit and in accordance with this policy as determined by the County, the County shall give written notice of the defects to the permittee at least thirty (30) days prior to the expiration date of any bond. If the corrective work has not been satisfactorily completed within twenty (20) days of the notice, the County shall exercise its right in utilizing the bond for correcting the deficiencies.

Public Safety Considerations

Work conducted by the permittee within the County right-of-way shall be conducted in such a manner to avoid creating obstructions or hazards to the traveling public. The permittee shall provide for the safety and convenience of the residents along roads where work is being performed, and for the protection of persons and property at all times.

- If access to the roadway will be disrupted as part of the proposed work, the permittee shall notify all parties who may be impacted in advance of the work.
- Materials and/or equipment necessary for construction shall not be stored in the County rights-of-way at any time unless approved in advance by the County. As work is being completed the roadway and roadside area where work has been performed shall be thoroughly cleared of all debris and remaining material and shall be restored to a condition equal to or better than the original when construction is concluded.
- Adequate warning signs, barricades, lighting, flagging and other devices as specified in the MUTCD, and as approved and required by the County, shall be provided, maintained, and paid for by the permittee. If flaggers are used or required, they shall be certified in accordance with WVDOT's policies.
- The permittee shall plan rights-of-way work so it does not create safety hazards or maintenance problems, render portions of rights-of-way unusable for future road improvement or obstruct major flood- ways.
- The permittee's operations shall conform to the applicable requirements established by the Federal Occupation Safety and Health Act (OSHA), and any other applicable laws or regulations.
- Staging of projects may be required by the County to produce the least disruption possible for the traveling public. Permission for any subsequent staging may not be allowed until the prior stage has satisfactorily progressed or been completed.

If, in the opinion of the County Road & Bridge Foreman or Superintendent, an unsafe condition exists, or the contractor is not in conformance with the approved traffic control plan, the County may suspend ALL operations until the situation is corrected. If the contractor does not remedy the situation immediately, the County may correct the problem and bill the contractor for any expenses incurred.

Responsibility for Rework

Any deficiencies identified during an inspection must be corrected by the permittee. All deficiencies shall be resolved to the satisfaction of the County at the property owner's or

permittee's expense. Failure to do so could be cause to deny acceptance of work performed and denial of future permits.

The permittee shall be notified by the inspector of any work or material which does not conform to these standards (e.g., pavement failures, substandard materials, broken asphalt, damaged signs, inadequate fencing, remaining debris, or improper drainage) and therefore must be corrected. Any required corrective work shall be initiated as soon as notice is given and done to the satisfaction of the County's inspector. Deficiencies identified and not sufficiently corrected during active construction may result in work stoppage on the project.

Expiration of Permits

Permits are valid for one (1) year after Board approval. If an extension is needed, a formal written request must be submitted to the Board for approval. If construction commenced and subsequently ceases or is abandoned for a period of six (6) months or more, a new permit shall be required before such work can be resumed; an extension may be allowed provided no changes in the original approved plans and specifications have been made or are required by the County.

Emergencies

If a true emergency exists, as determined by the County, where time is not available to follow the procedures for obtaining a permit or for making modifications to the approved permit, specifications and schedules, an entity may, after receipt of approval by the Road & Bridge Foreman, proceed with the work. Within the first two business days following the emergency, the entity shall apply for a Right-of-Way Permit or secure approval for the necessary changes to previously approved plans and schedules.

Permit Work Suspension or Revocation

The County may suspend or revoke any permit, in writing, issued under the provisions of this policy for the following reasons:

- If the permit is issued in error or based on incorrect information supplied by the applicant.
- When the permittee is not in compliance with the permit conditions.
- A hazard is created which would pose a threat to the health, safety, and welfare of the public.

Should a Right-of-Way Permit be revoked or suspended, all work shall be suspended until a new permit is issued or work under the revoked or suspended permit is re-authorized by the County. The County may remove any hazard determined not to be in compliance with permit conditions. Any cost incurred by the County shall be billed to the permittee in violation.

Road Closure Procedure

Road closures are not permitted unless justified on the basis of overall benefit to the general public; however, the County recognizes there are times when the need to close a road arises. Closure of intersections must be avoided whenever possible and is not recommended for overnight or weekend/holiday closures. No road closures shall be undertaken unless approved by the Board; the extent, time, location, and frequency of closures is at the discretion of the Board. Any proposed road closure must be submitted in writing to the County Road and Bridge Superintendent. If a proposed road closure is associated with a Right-of-Way Permit, the written request must be included as part of the application materials. Planned closures of County roads for construction or repairs of a road or a bridge by the Road and Bridge Department will be publicized in the local newspaper and posted on the County's website (www.carbonwy.com) before construction commences.

Guidelines for road closure requests follow.

Types of Road Closure Events

A. Simple Event Closure

1. Closure for non-complicated event (for a duration of less than eight (8) hours in a single day)
2. Permitted only between the hours of 8:00am and 5:00pm (unless otherwise authorized, in writing, by the County).
3. Written request to be submitted a minimum of ten (10) business days prior to the anticipated need for the closure.
4. All written requests must contain the following information:
 - The location of the proposed closure including the road(s) expected to be impacted.
 - Reason and time frame of the anticipated closure.
 - A sketch plan/diagram of work to be completed.
 - A traffic control plan conforming to the requirements of the MUTCD, to include a detour plan (if necessary) or plan to maintain access for local residents, school buses, postal delivery vehicles and all emergency services (including but not limited to Sheriff's Department, local Police Department, Fire District response vehicles, Search & Rescue and Highway Patrol).
 - List of names and phone numbers of a minimum of two (2) responsible persons.

B. Complex Event Closure

1. Closure for complicated events (for a duration of more than 8 hours and/or multiple days).
2. Written request to be submitted a minimum of twenty (20) business days prior to the anticipated need for the closure.
3. All written requests must contain the following information:
 - Justification for proposing a complex event closure versus utilizing a simple event closure.
 - The location of the proposed closure including the road(s) expected to be impacted.
 - A sketch plan/diagram of work to be completed.
 - Estimated time frame of the anticipated closure, including the daily schedule of closure times and when construction may be suspended. Overnight and weekend/holiday closures are not recommended.
 - A traffic control plan conforming to the requirements of the MUTCD, to include a detour plan (if necessary) or plan to maintain access for local residents, school buses, postal delivery vehicles and all emergency services (including but not limited to Sheriff's Department, local Police Department, Fire District response vehicles, Search & Rescue and Highway Patrol).

Speed Limits

All County roads have a set speed limit according to Wyoming State Statutes. Current default speed limits on all paved County roads are set at sixty-five (65) miles per hour and graveled roads are set at fifty-five (55) miles per hour, unless posted otherwise. It is the policy of the County to follow Wyoming State Statutes and WYDOT's *Procedures for Locally Establishing Speed Limits* in establishing any future speed zones lower than the default speed limit.

Establishment, Vacation or Alteration of County Roads

Any party wishing to establish, vacate or alter a road or easement must conform to the statutory requirements of W.S. §24-3-101,*et seq.* There are several subdivision roads and other prior existing

roads within Carbon County which are not County roads. In W.S. §24-3-101, *et seq.*, there is a statutorily prescribed process to be followed to petition the County to establish a road as a County Road, as well as guidance for altering or vacating County roads. The Board may grant or deny establishment, vacation, or alteration of these roads. Prior to the establishment of a County Road, the County may require the road to be brought up to these prescribed standards.

Subdivision Roads

Where roads are proposed as part of a subdivision, the roads shall be built to County standards. The developer may propose to build either public or private roads and the County may require that roads be offered for dedication to the public. Roads that are dedicated to the public are not considered County roads unless established as outlined in this policy. The County does not, as a general rule, adopt or maintain any roads within a subdivision or development.

Enforcement of Design and Construction Standards

The Road and Bridge Superintendent or County Road foreman is authorized to enforce all provisions of this Chapter. The Board may appoint other agents, persons with expertise and technical training, inspecting officers or designated employees to act as an authorized representative. Whenever any work is proceeding contrary to the provisions of these standards, the County may order the work stopped by a written notice which shall be served on any persons engaged in the doing or causing of such work to be done, and any such persons shall forthwith stop such work until authorized by the County to proceed.

Liability

Carbon County, or its authorized representative charged with the enforcement of this policy, acting in good faith and without malice in the discharge of duties set forth herein, shall not be rendered liable for any damage that may occur to persons or property as a result of any act or by reason of any act or omission in the discharge of such duties.

The County assumes no liability for destruction or compromising of any utility or structures within County rights-of-way that may be encountered during normal maintenance of County roads. Utilities assume all responsibility for marking their installation within County rights-of-way. County will endeavor to locate all utilities prior to construction or maintenance that may encounter a utility but will not be held responsible for mismarked or accidental damage.

Carbon County does not waive sovereign immunity by entering into this Road and Bridge Management Policy and specifically retains immunity and all defenses available to it as a governmental entity pursuant to Wyoming Statutes and all other applicable State and Federal laws.

Violations

It shall be unlawful for any person, firm, governmental agency, or corporation to construct, enlarge, alter, repair, move, improve, remove, excavate, convert, or demolish any public improvements or common facilities or permit the same to be done in violation of this policy. Any individual who violates these standards shall be subject to denial of future Right-of-Way Permits and may be subject to fines or other penalties, as set out herein.

Penalties

Any individual or entity found to have performed or executed any installation within a County right-of-way as determined by the Road and Bridge Foreman or the County Superintendent, without first obtaining an approved permit and following proper procedures outlined within said permit and this policy shall be liable for a penalty of \$750 per calendar day for each day the violation continues, plus all costs necessary to remediate or correct the non-compliance. A citation for non-compliance with an approved permit or this policy may be issued by the County Sheriff and prosecuted by the County Attorney.

Carbon County
Road & Bridge Department
Fee Schedule Summary

Please see individual permits for additional details/further explanation

Oversize/Overweight Permits

\$50.00 Class A Oversize

\$40.00 Class A Overweight

+\$1/ton/mile for loads exceeding 60,000lbs.

\$75.00 base Administrative Fee for Route Analysis for loads in excess of 160,000 lbs.

\$75.00/Hr./person Personnel Time Fee for loads requiring Sheriff Escort

\$25.00 Class B, C, Prior-Operation, & Off-load, Oversize

\$40.00 Class B, C, Prior-Operation, & Off-load, Overweight

\$50.00 Class D Annual, Oversize, Restrictions Apply

\$450.00 Class D Annual, Overweight, Restrictions Apply

\$15.00 (single), \$50.00 (extended) Class F, Restrictions Apply

\$450.00 Class W, Restrictions Apply

\$4500.00 Class R (Rig Move)

\$7.35 Fee for mailing up to 50 self-issue Permits, self-issue authority required

\$7.80 Fee for mailing 50+ self-issue Permits, self-issue authority required

\$100.00 per permit audit penalty for missing Class B self-issue Permits

Approach/Access/Road Construction/Sign/Mailbox/Other Permits

\$100 Application Fee for permanent structure - \$25.00 for temporary structure

\$100 Inspection Fee for locations within twenty (20) air mile radius of Road & Bridge Office

\$150 Inspection Fee for locations greater than twenty (20) air mile radius of Road & Bridge Office

Snow Removal Permits

\$25.00 Fee for daily permits

\$500.00 fee for seasonal permits – (requires yearly renewal)

Bore Permits

\$125 Bore Fee per hole

\$100 Inspection Fee for locations within twenty (20) air mile radius of Road & Bridge Office

\$150 Inspection Fee for locations greater than twenty (20) air mile radius of Road & Bridge Office

Temporary Access Addendum to Bore Permits

\$400 Permit Application Fee

\$100 Inspection Fee for locations within twenty (20) air mile radius of Road & Bridge Office

\$150 Inspection Fee for locations greater than twenty (20) air mile radius of Road & Bridge Office

\$400 Renewal Fee (requires completion minimum of 10 days prior to expiration – no additional

inspections required)

Public Utilities Parallel Right of Way Permit

\$100 minimum fee Parallel Right-of-Way (see permit for further detail)

\$100 minimum fee just for crossing (each crossing is an additional \$100.00)

\$100 Inspection Fee could be added for locations within twenty (20) air mile radius of Road & Bridge Office

\$150 Inspection Fee could be added for locations greater than twenty (20) air mile radius of Road & Bridge Office

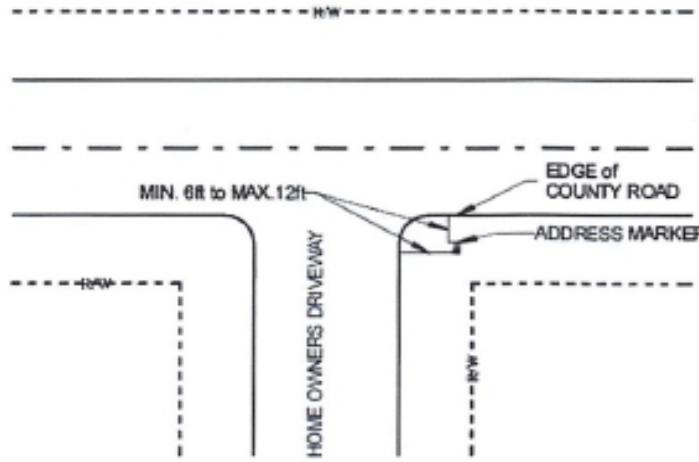
Carbon County Road & Bridge – PO Box 487,
1016 West Hugus, Rawlins, WY 82301

By Telephone: (307)
324-9555

By Fax: (307) 324-9557

By Email: kandisfritz@carbonwy.com

**COUNTY ADDRESS
MARKER PLACEMENT DETAILS
on COUNTY ROADS**

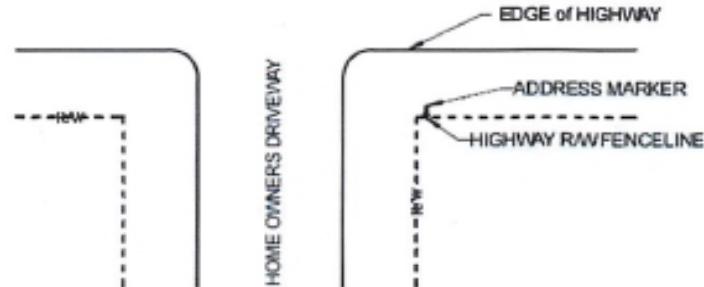


T-POST NOTES

1. T-posts shall be 5-foot studded steel, 1.25lb. per foot, painted green.
2. Address Markers shall be attached by either 1/4" rivets or 1/4" bolts and nuts.

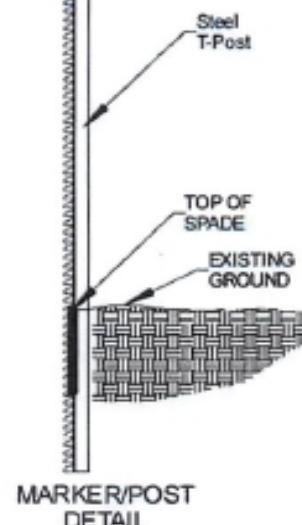


**COUNTY ADDRESS
MARKER PLACEMENT DETAILS
on HIGHWAYS**



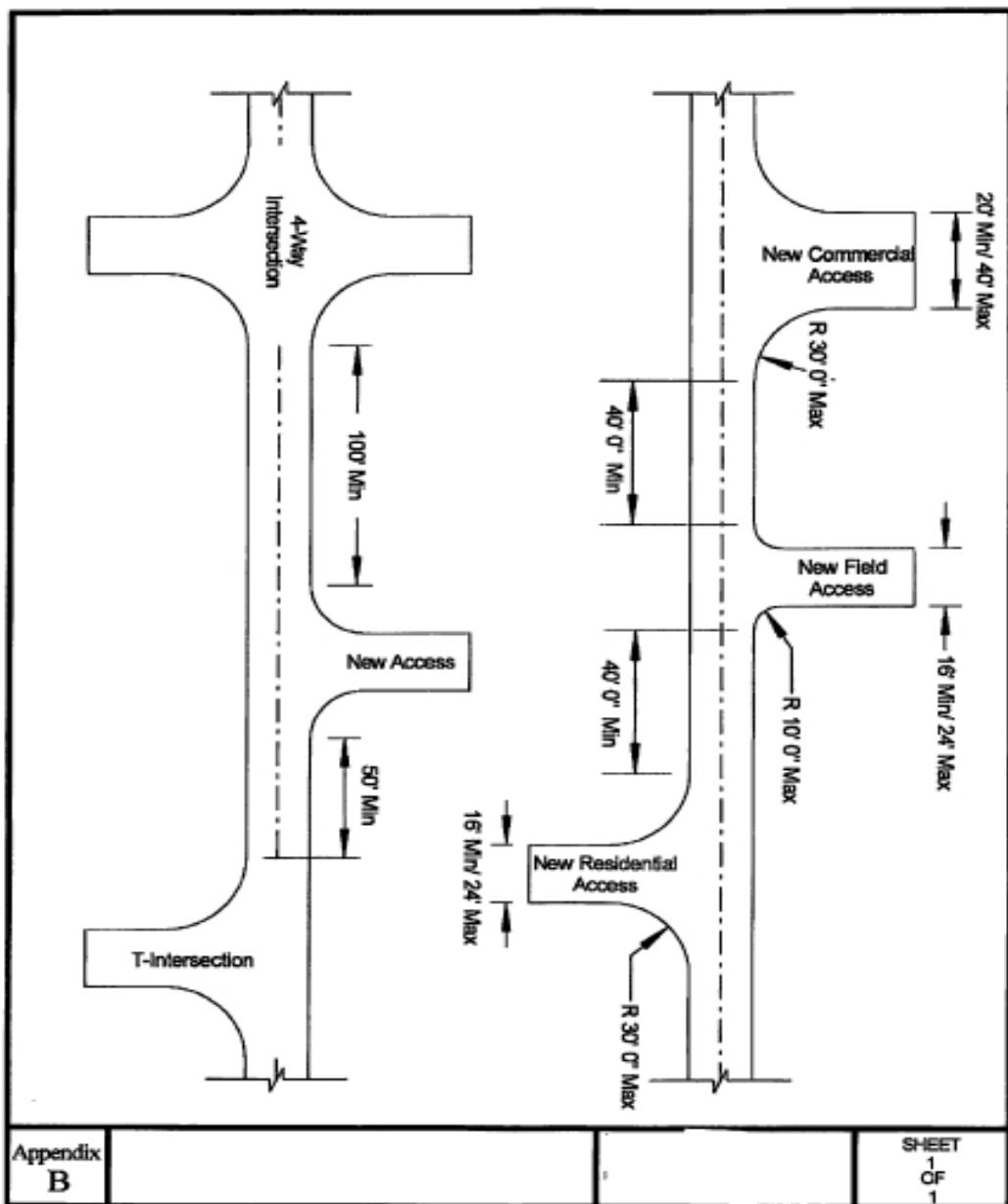
**MARKER PLACEMENT
DETAIL**

**TYPICAL DRAWINGS
(NOT TO SCALE)**



**MARKER/POST
DETAIL**

APPENDIX B: TYPICAL ACCESS LOCATION DRAWING

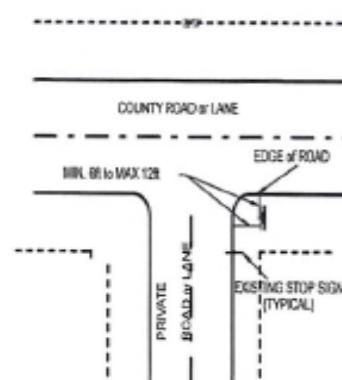


APPENDIX D: TYPICAL ROAD SIGN DETAIL

GENERAL NOTES

1. All signs, posts and installation of same shall conform to the latest Edition of the Manual on Uniform Traffic Control Devices (MUTCD).

PRIVATE ROAD SIGN PLACEMENT D. ETALIS

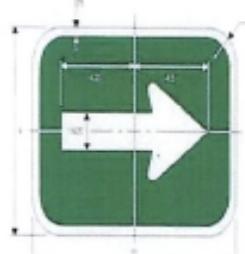


TYPICAL DRAWINGS (NOT TO SCALE)

PRIVATE ROAD SIGN



DIRECTIONAL ARROWS



1. Background shall be Green Reflective and Lettering shall be White Reflective.
2. Upper Case Letters shall be Four Inches (4"), those that are supporting letters shall be Two Inches (2").
3. Private signs shall have a White Reflective Border.

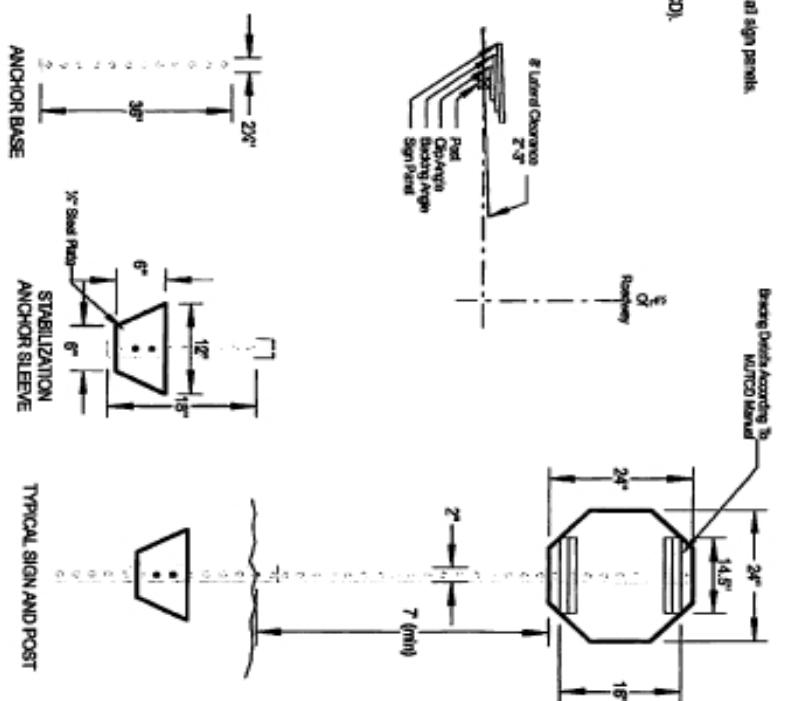
1. Type A, Background shall be Blue Reflective, with the Arrow and Border being Yellow Reflective.
2. Type B, Background shall be Green Reflective, with the Arrow and Border being White Reflective.

GENERAL NOTES.

1. Diamond grade reflection sheeting is required for all sign panels.
2. All signs and post installation of same shall conform to the latest Edition of the Manual on Uniform Traffic Control Devices (MUTCD).

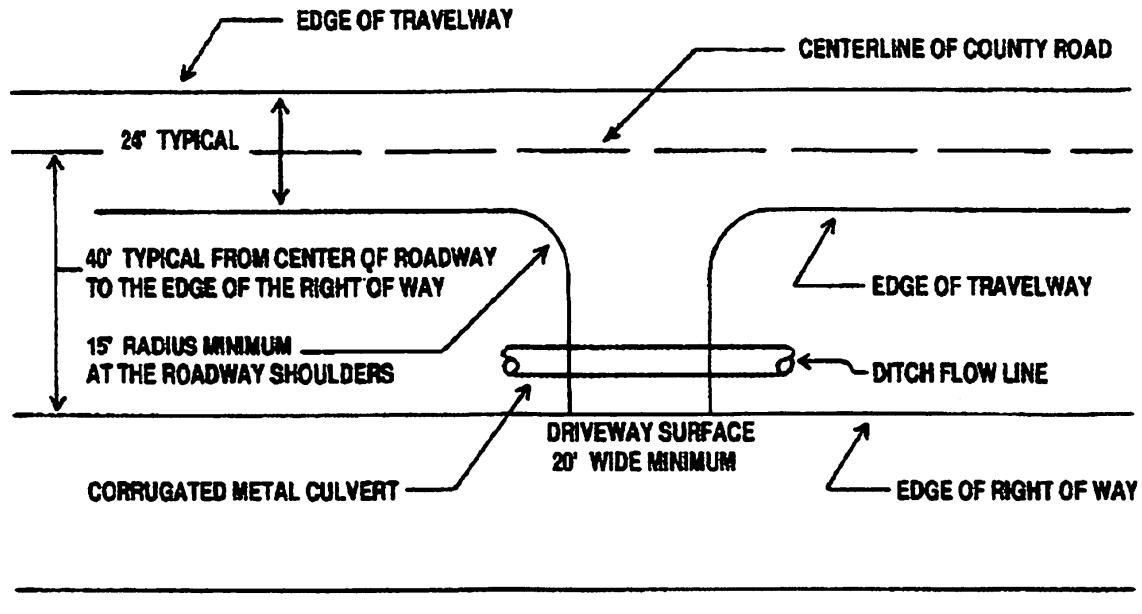
SHIM POST & ANCHOR BASE

- Post shall be 12ga. and 2" Performed.
- Post/Anchor Bases shall be Driven to a minimum depth of 3' with Stabilization Anchors Securely Attached.
- 1" to 2" of the Anchor Bases shall be left above the surface.
- A min. of One Post (1') of the Post shall be installed into the Anchor Bases and secured using 5/16" hex head nuts & bolts.
- Signs shall be attached to the posts using a minimum of two 5/16" hex head bolts, nuts & washers.

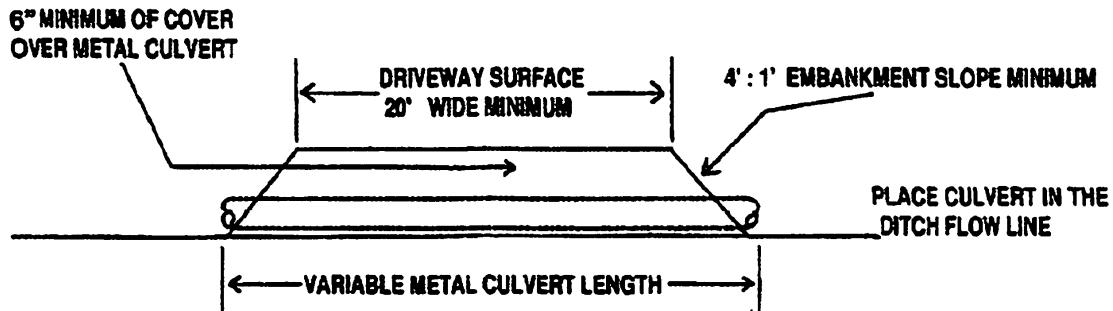


Typical sign and post

STANDARD DESIGN



TYPICAL CROSS SECTION



(CULVERT LENGTH DEPENDS ON THE WIDTH OF THE DRIVEWAY SURFACE AND THE DEPTH OF THE DITCH)

NOTE : (DRIVEWAY SURFACE)
3" OF CRUSHED ROAD BASE MINIMUM